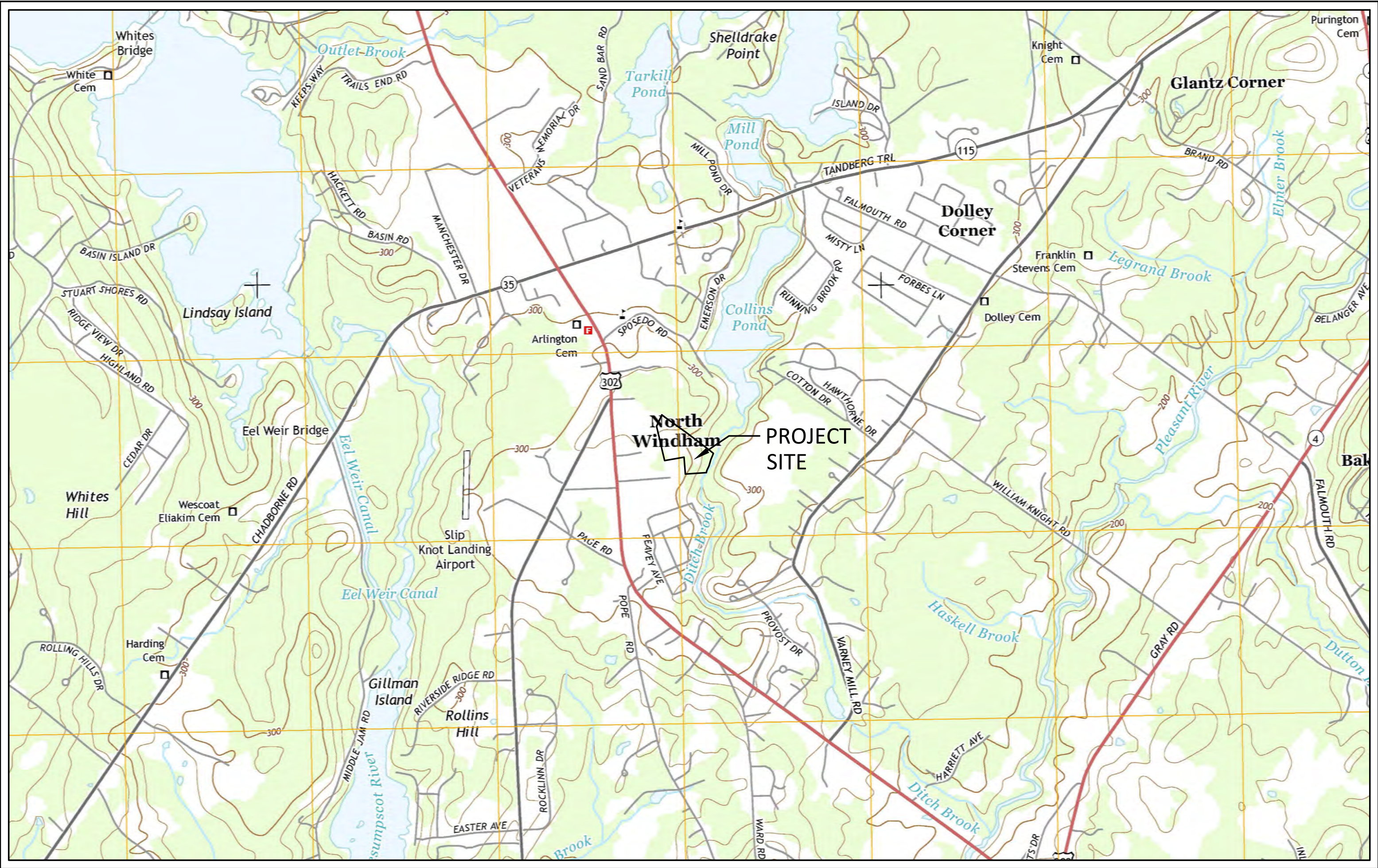


TURNING LEAF HEIGHTS

TURNING LEAF DRIVE & DRIVE-IN LANE
WINDHAM, MAINE

CONSULTANTS	
CIVIL ENGINEER	DM ROMA CONSULTING ENGINEERS
LAND SURVEYOR	SURVEY, INC.
SOIL SCIENTIST	MARK HAMPTON ASSOCIATES, INC.
WETLAND SCIENTIST	MAINELY SOILS, LLC



PROJECT VICINITY MAP

ISSUED FOR PERMITTING - NOT FOR CONSTRUCTION
MAY 18, 2026

PREPARED BY:

DM ROMA

CONSULTING ENGINEERS
P.O. BOX 1116
WINDHAM, ME 04062
(207) 591-5055

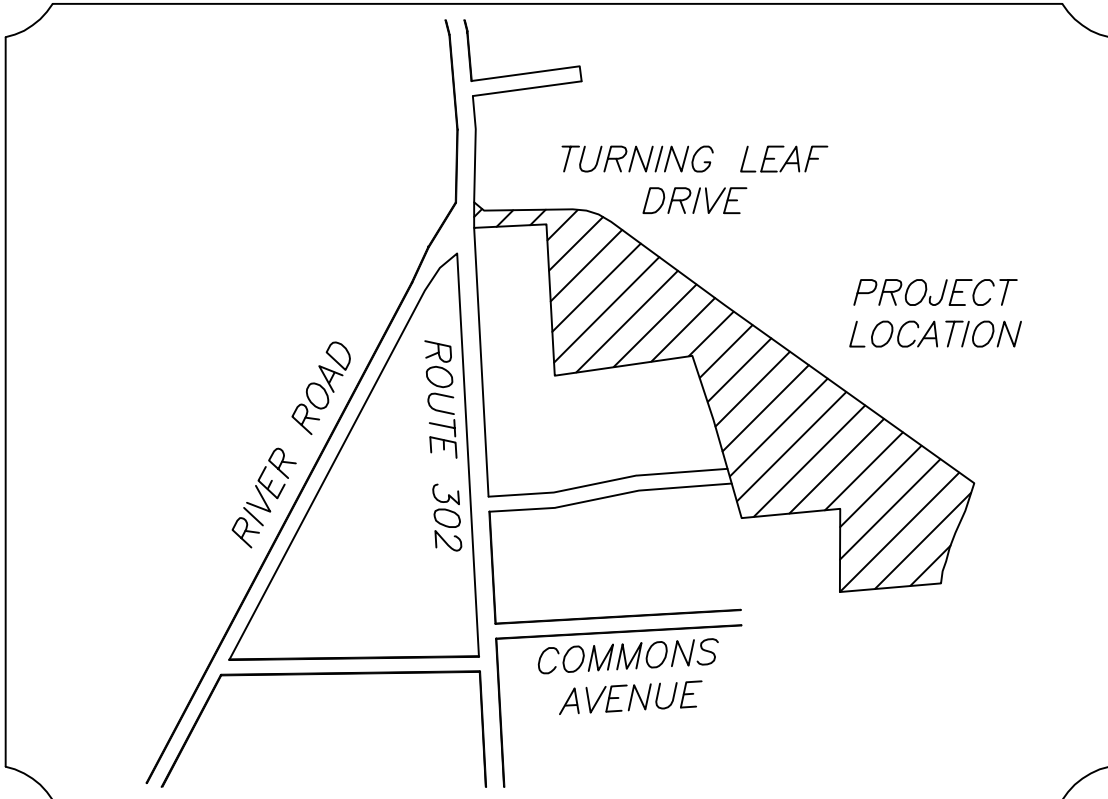
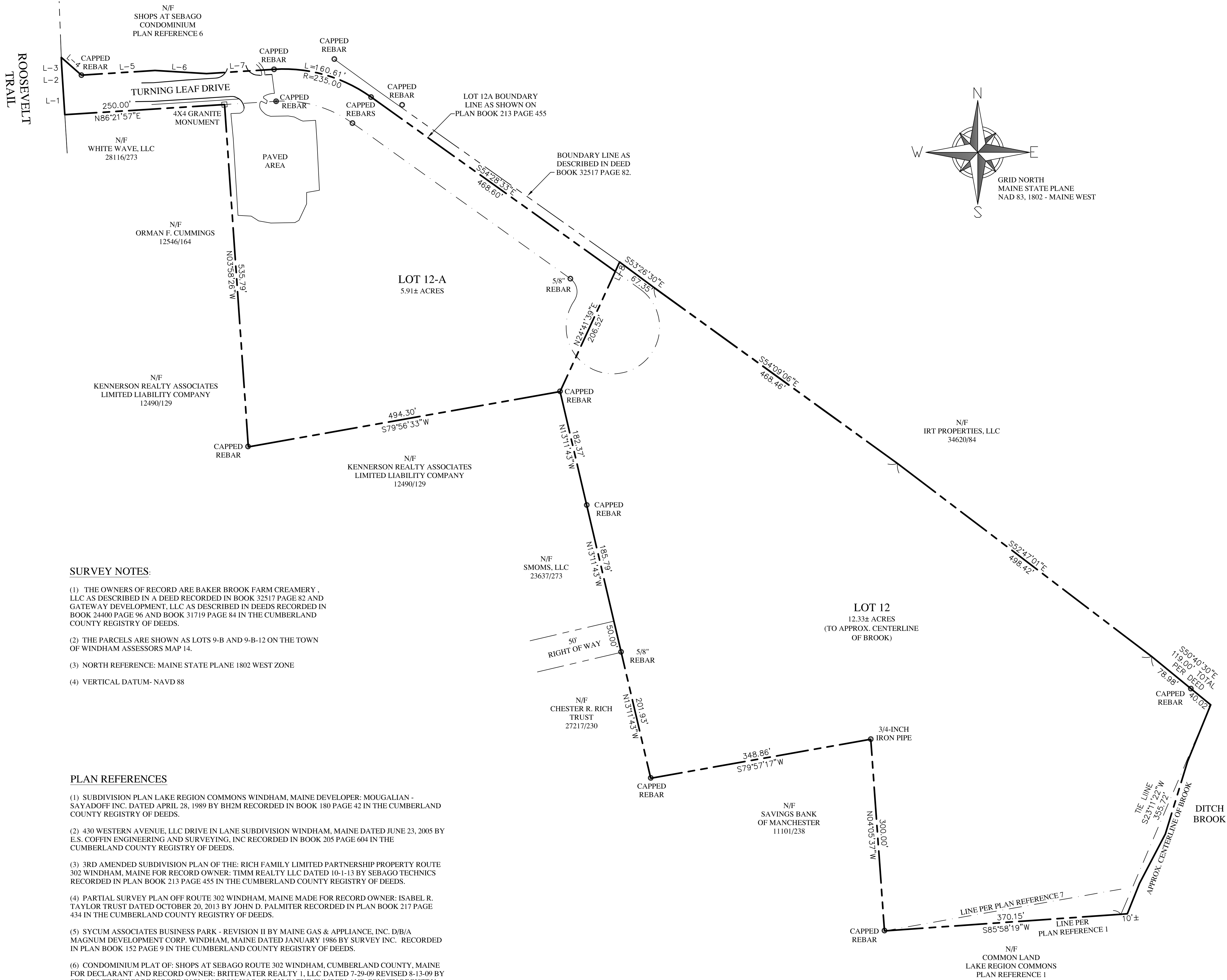
APPLICANT:
GATEWAY DEVELOPMENT, LLC
4 COMMONS AVENUE - SUITE 12
WINDHAM, MAINE 04062

TURNING LEAF HEIGHTS DRAWING SHEET INDEX

PAGE NO.	DESCRIPTION
1	TITLE SHEET
2	BOUNDARY SURVEY
3	EXISTING CONDITIONS PLAN
4	SUBDIVISION PLAN
5	OVERALL DEVELOPMENT PLAN
6	SITE & LANDSCAPING PLAN - LOT 1
7	SITE & LANDSCAPING PLAN - LOT 2 & 5
8	SITE & LANDSCAPING PLAN - LOT 3
9	SITE & LANDSCAPING PLAN - LOT 4
10	GRADING AND UTILITY PLAN
11	GRADING AND UTILITY PLAN
12	ROADWAY PLAN AND PROFILE - TURNING LEAF DRIVE
13	ROADWAY PLAN AND PROFILE - TURNING LEAF DRIVE
14	ROADWAY PLAN AND PROFILE - TURNING LEAF DRIVE
15	ROADWAY PLAN AND PROFILE - DRIVE-IN LANE
16	DETAILS
17	DETAILS
18	DETAILS
19	DETAILS

PERMITTING PLAN ATTACHMENTS

HIGH-INTENSITY SOIL SURVEY
WATERSHED MAP: EXISTING CONDITION
WATERSHED MAP: PROPOSED CONDITION



LINE TABLE

L-1	N03°38'03"W	60.00'
L-2	N03°38'34"W	19.00'
L-3	N02°10'50"W	10.64'
L-4	S48°38'03"E	41.91'
L-5	N86°21'57"E	115.08'
L-6	S86°30'38"E	80.64'
L-7	N86°21'57"E	105.62'
L-8	N24°43'18"E	16.04'

SURVEY NOTES:

- THE OWNERS OF RECORD ARE BAKER BROOK FARM CREAMERY, LLC AS DESCRIBED IN A DEED RECORDED IN BOOK 32517 PAGE 82 AND GATEWAY DEVELOPMENT, LLC AS DESCRIBED IN DEEDS RECORDED IN BOOK 24400 PAGE 96 AND BOOK 31719 PAGE 84 IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS.
- THE PARCELS ARE SHOWN AS LOTS 9-B AND 9-B-12 ON THE TOWN OF WINDHAM ASSESSORS MAP 14.
- NORTH REFERENCE: MAINE STATE PLANE 1802 WEST ZONE
- VERTICAL DATUM- NAVD 88

PLAN REFERENCES

- SUBDIVISION PLAN LAKE REGION COMMONS WINDHAM, MAINE DEVELOPER: MOUGALIAN - SAYADOFF INC. DATED APRIL 28, 1989 BY BH2M RECORDED IN BOOK 180 PAGE 42 IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS.
- 430 WESTERN AVENUE, LLC DRIVE IN LANE SUBDIVISION WINDHAM, MAINE DATED JUNE 23, 2005 BY E.S. COFFIN ENGINEERING AND SURVEYING, INC RECORDED IN BOOK 205 PAGE 604 IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS.
- 3RD AMENDED SUBDIVISION PLAN OF THE: RICH FAMILY LIMITED PARTNERSHIP PROPERTY ROUTE 302 WINDHAM, MAINE FOR RECORD OWNER: TIMM REALTY LLC DATED 10-1-13 BY SEBAGO TECHNICS RECORDED IN PLAN BOOK 213 PAGE 455 IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS.
- PARTIAL SURVEY PLAN OFF ROUTE 302 WINDHAM, MAINE MADE FOR RECORD OWNER: ISABEL R. TAYLOR TRUST DATED OCTOBER 20, 2013 BY JOHN D. PALMITER RECORDED IN PLAN BOOK 217 PAGE 434 IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS.
- SYCAM ASSOCIATES BUSINESS PARK - REVISION II BY MAINE GAS & APPLIANCE, INC. D/B/A MAGNUM DEVELOPMENT CORP. WINDHAM, MAINE DATED JANUARY 1986 BY SURVEY INC. RECORDED IN PLAN BOOK 152 PAGE 9 IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS.
- CONDOMINIUM PLAT OF: SHOPS AT SEBAGO ROUTE 302 WINDHAM, CUMBERLAND COUNTY, MAINE FOR DECLARANT AND RECORD OWNER: BRITTEWATER REALTY I, LLC DATED 7-29-09 REVISED 8-13-09 BY SEBAGO TECHNICS RECORDED IN PLAN BOOK 209 PAGE 232 IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS.
- PLAN OF PROPERTY IN WINDHAM, MAINE FOR MAGNUM DEVELOPMENT, CORP DATED JULY 1985 BY SURVEY INC.

CERTIFICATION:

I CERTIFY THAT THIS SURVEY CONFORMS TO THE STANDARDS OF THE MAINE BOARD OF LICENSURE FOR PROFESSIONAL LAND SURVEYORS AND IS CORRECT TO THE BEST OF MY KNOWLEDGE, INFORMATION AND BELIEF.

WILLIAM C. SHIPPEN P.L.S. 2118

BOUNDARY & TOPOGRAPHIC SURVEY

TURNING LEAF DRIVE
WINDHAM, ME

FOR: **BAKER BROOK FARM CREAMERY, LLC**
GATEWAY DEVELOPMENT, LLC

TURNING LEAF DRIVE
WINDHAM, MAINE 04062
(OWNERS OF RECORD)

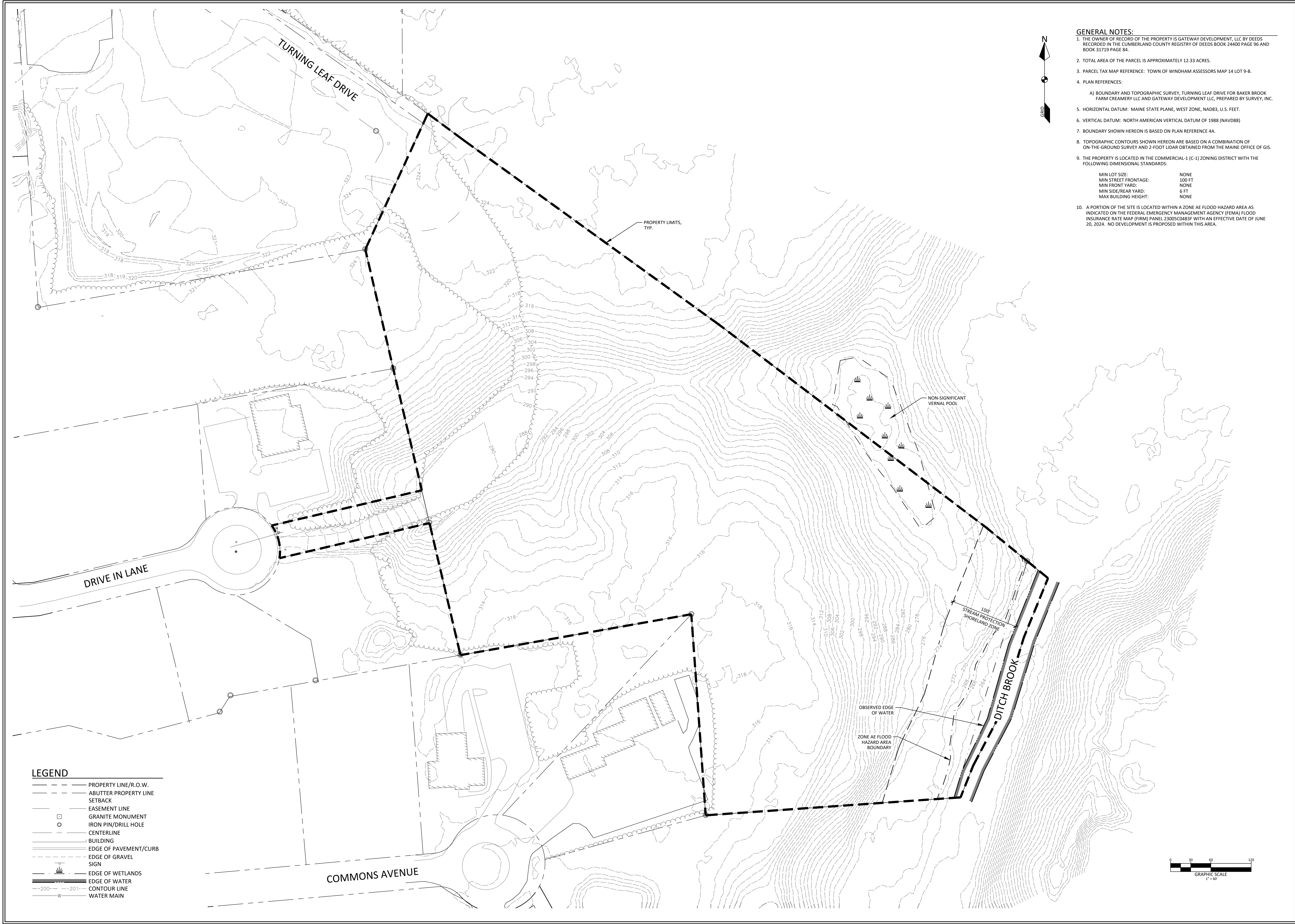
SURVEY BY: **SURVEY, INC.**

P.O. BOX 210
WINDHAM, ME 04062

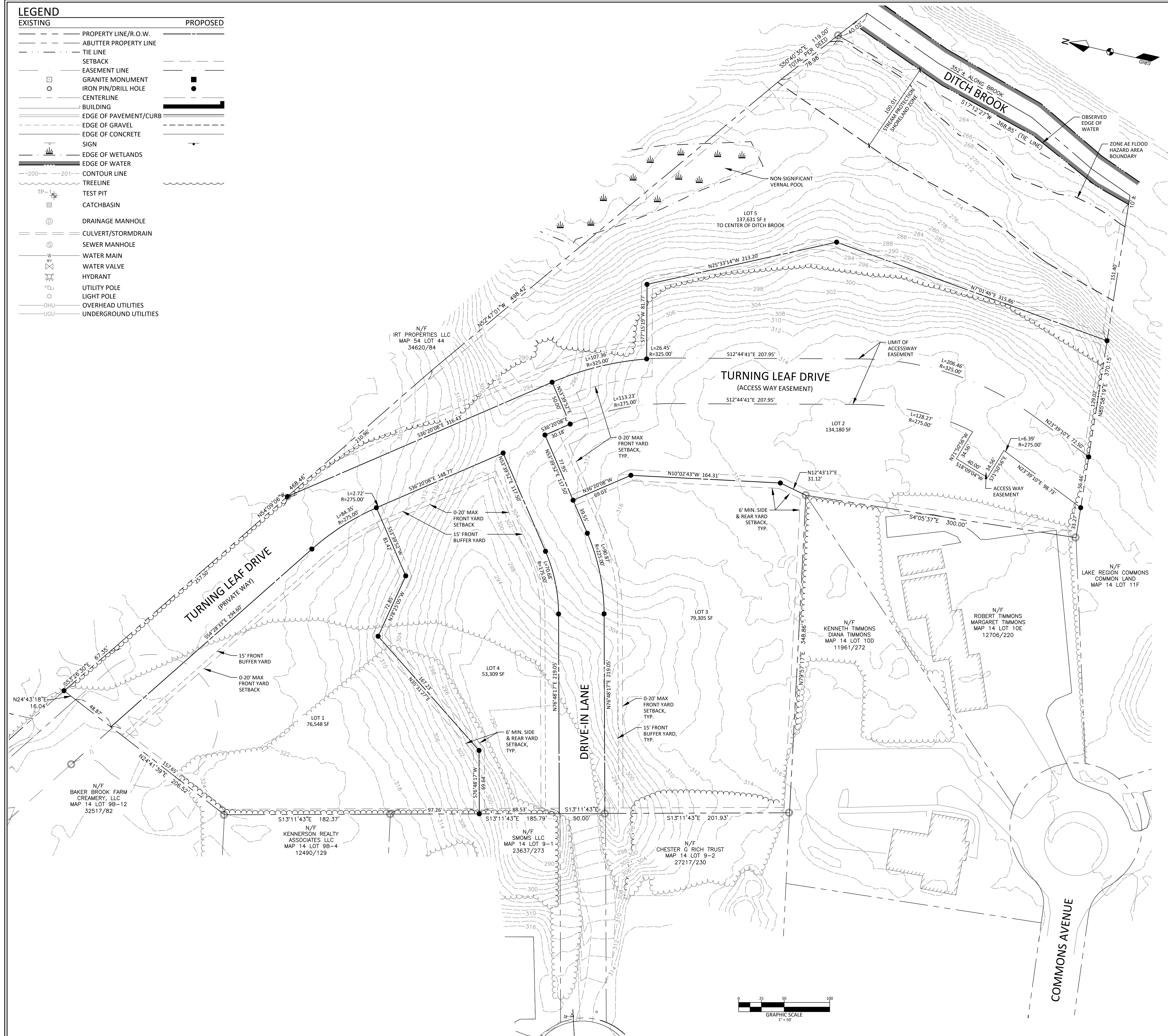
(207) 892-2556
INFO@SURVEYINCORPORATED.COM

DWN: DRR
DATE: APRIL 2023
REVISED: JANUARY 2026 ADD LOT AREAS, CL OF BROOK, DEED REF.

CHK: WCS
JOB NO. 23-054



EXISTING		PROPOSED
	PROPERTY LINE/R.O.W.	
	ABUTTER PROPERTY LINE	
	TIE LINE	
	SETBACK	
	EASEMENT LINE	
	GRANITE MONUMENT	
	IRON PIN/DRILL HOLE	
	CENTERLINE	
	BUILDING	
	EDGE OF PAVEMENT/CURB	
	EDGE OF GRAVEL	
	EDGE OF CONCRETE	
	SIGN	
	EDGE OF WETLANDS	
	EDGE OF WATER	
	CONTOUR LINE	
	TREELINE	
	TEST PIT	
	CATCHBASIN	
	DRAINAGE MANHOLE	
	CULVERT/STORM DRAIN	
	SEWER MANHOLE	
	WATER MAIN	
	WATER VALVE	
	HYDRANT	
	UTILITY POLE	
	LIGHT POLE	
	OVERHEAD UTILITIES	
	UNDERGROUND UTILITIES	



GROSS LAND AREA OF SUBDIVISION:	537,243± S.F. (12.33± ACRES)
DEDUCTIONS:	
1. RIGHT-OF-WAY	56,270 S.F.
2. STEEP SLOPES (OVER 25%)	86,050 S.F.
3. 100-YEAR FLOOD PLAIN	16,652 S.F.
4. RESOURCE PROTECTION DISTRICT	0 S.F.
5. VERY POORLY DRAINED SOILS	0 S.F.
6. SURFACE WATERBODIES	0 S.F.
7. SIGNIFICANT WILDLIFE HABITAT	0 S.F.
8. ENDANGERED BOTANICAL RESOURCES	0 S.F.
TOTAL NET AREA OF SUBDIVISION PARCEL:	378,271± S.F. (8.68± ACRES)

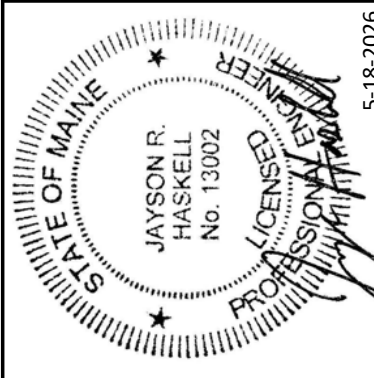
1. THE OWNER OF RECORD OF THE PROPERTY IS GATEWAY DEVELOPMENT, LLC, BY DEEDS RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK 24400 PAGE 96 AND BOOK 31719 PAGE 84.
2. TOTAL AREA OF THE PARCEL IS APPROXIMATELY 12.33 ACRES.
3. PARCEL TAX MAP REFERENCE: TOWN OF WINDHAM ASSESSORS MAP 14 LOT 9-B.
4. PLAN REFERENCES:
 - A) BOUNDARY AND TOPOGRAPHIC SURVEY, TURNING LEAF DRIVE FOR BAKER BROOK FARM CREAMERY LLC AND GATEWAY DEVELOPMENT LLC, PREPARED BY SURVEY, INC.
5. HORIZONTAL DATUM: MAINE STATE PLANE, WEST ZONE, NAD83, U.S. FEET.
6. VERTICAL DATUM: NORTH AMERICAN VERTICAL DATUM OF 1988 (NAD88)
7. BOUNDARY SHOWN HEREON IS BASED ON PLAN REFERENCE 4A.
8. TOPOGRAPHIC CONTOURS SHOWN HEREON ARE BASED ON A COMBINATION OF ON-THE-GROUND SURVEY AND 2 FOOT LIDAR.
9. THE PROPERTY IS LOCATED IN THE COMMERCIAL-1 (C-1) ZONING DISTRICT WITH THE FOLLOWING DIMENSIONAL STANDARDS:

MIN LOT SIZE:	NONE
MIN STREET FRONTAGE:	100 FT
MIN FRONT YARD:	0 TO 20 FT
MIN SIDE/REAR YARD:	6 FT
MAX BUILDING HEIGHT:	75 FT
10. A PORTION OF THE SITE IS LOCATED WITHIN A ZONE A+ FLOOD HAZARD AREA AS INDICATED ON THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) FLOOD INSURANCE RATE MAP (FIRM) PANEL 2300SC0483F WITH AN EFFECTIVE DATE OF JUNE 20, 2024. NO DEVELOPMENT IS PROPOSED WITHIN THIS AREA.
11. THIS PROJECT IS SUBJECT TO A SITE LOCATION OF DEVELOPMENT ACT (SLODA - L-31404-87-A-N) FROM THE MAINE DEPARTMENT OF ENVIRONMENTAL PROTECTION.
12. ANY TREE NOT BEING REMOVED BEYOND THE LIMITS OF TREE CLEARING SHOWN ON THIS PLAN SHALL NOT BE PERMITTED FOR A PERIOD OF 5 YEARS FOLLOWING THE PLANNING BOARD APPROVAL DATE PRINTED ON THIS PLAN, UNLESS AN AMENDMENT IS APPROVED BY THE PLANNING BOARD.
13. ALL ROADS IN THE SUBDIVISION SHALL REMAIN PRIVATE ROADS TO BE MAINTAINED BY THE DEVELOPER, LOT OWNERS OR ROAD ASSOCIATION, AND SHALL NOT BE OFFERED FOR ACCEPTANCE, OR MAINTAINED, BY THE TOWN OF WINDHAM UNTIL THE ROAD MEETS ALL MUNICIPAL STREET DESIGN AND CONSTRUCTION STANDARDS APPLICABLE TO PUBLIC STREETS.

CHAIRPERSON	DATE

STATE OF MAINE	
_____ COUNTY SS. REGISTRY OF DEEDS	
RECEIVED _____, 20____	
AT ____h ____m ____M AND RECORDED IN	
PLAN BOOK _____ PAGE _____	
ATTEST: _____	REGISTER

PERMITTING PLAN
NOT FOR
CONSTRUCTION



DMROMA
CONSULTING ENGINEERS

REV	DATE	BY	DESCRIPTION
A	9-25-24	JRH	ISSUED FOR MDEP PERMIT REVIEW
B	7-7-25	JRH	ISSUED FOR TOWN REVIEW
C	11-17-25	JRH	REVISED PER TOWN REVIEW COMMENTS
D	2-5-26	DMR	REVISED PER TOWN REVIEW COMMENTS
E	5-18-26	DMR	FINAL PLAN REVIEW

SUBDIVISION PLAN

TURNING LEAF HEIGHTS
TURNING LEAF DRIVE, DRIVE IN LANE & COMMONS AVENUE
WINDHAM, MAINE

FOR RECORD OWNER:

GATEWAY DEVELOPMENT, LLC
4 COMMONS AVENUE - SUITE 12
WINDHAM, ME 04062

20013
JOB NUMBER:

1" = 50'

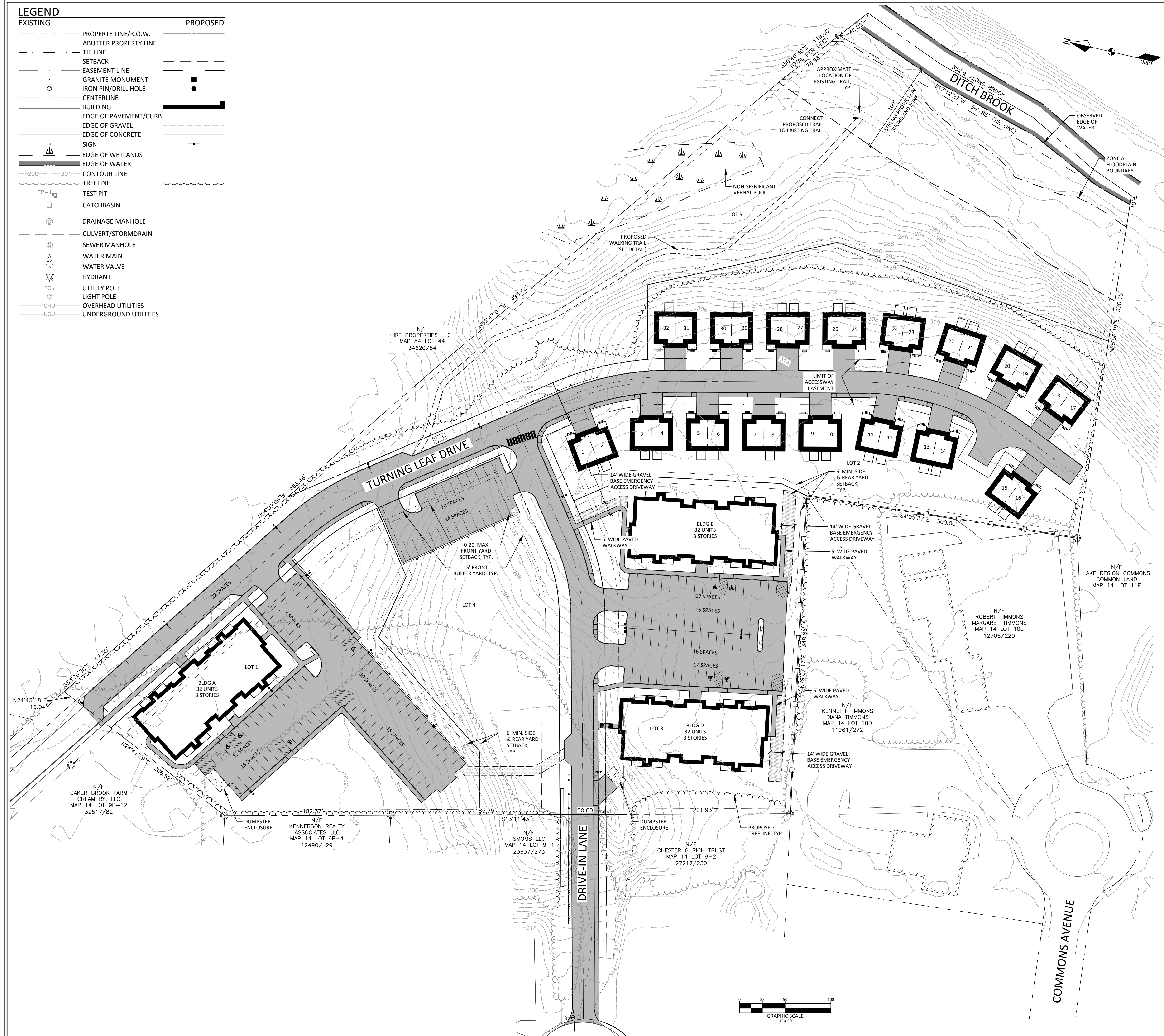
SCALE:

5-18-2026

SHEET 4 OF 19

SB-1

EXISTING		PROPOSED	
—————	PROPERTY LINE/R.O.W.	—————	
—————	ABUTTER PROPERTY LINE	—————	
—————	TIE LINE	—————	
—————	SETBACK	—————	
—————	EASEMENT LINE	—————	
□	GRANITE MONUMENT	●	
○	IRON PIN/DRILL HOLE	●	
—————	CENTERLINE	—————	
—————	BUILDING	—————	
=====	EDGE OF PAVEMENT/CURB	=====	
=====	EDGE OF GRAVEL	=====	
=====	EDGE OF CONCRETE	=====	
—————	SIGN	—————	
	EDGE OF WETLANDS	—————	
=====	EDGE OF WATER	=====	
---200--- ---201---	CONTOUR LINE	—————	
~~~~~	TREE LINE	~~~~~	
TP=	TEST PIT	—————	
■	CATCHBASIN	—————	
⊙	DRAINAGE MANHOLE	—————	
=====	CULVERT/STORMDRAIN	=====	
⊙	SEWER MANHOLE	—————	
—W—	WATER MAIN	—————	
—WV—	WATER VALVE	—————	
	HYDRANT	—————	
⊙	UTILITY POLE	—————	
☆	LIGHT POLE	—————	
—OHU—	OVERHEAD UTILITIES	—————	
—UGU—	UNDERGROUND UTILITIES	—————	



NET RESIDENTIAL AREA CALCULATIONS:

GROSS LAND AREA OF SUBDIVISION: 537,243± S.F. (12.33± ACRES)

DEDUCTIONS:

- |                                   |             |
|-----------------------------------|-------------|
| 1. RIGHT-OF-WAY                   | 56,270 S.F. |
| 2. STEEP SLOPES (OVER 25%)        | 86,050 S.F. |
| 3. 100-YEAR FLOOD PLAIN           | 16,652 S.F. |
| 4. RESOURCE PROTECTION DISTRICT   | 0 S.F.      |
| 5. VERY POORLY DRAINED SOILS      | 0 S.F.      |
| 6. SURFACE WATERBODIES            | 0 S.F.      |
| 7. SIGNIFICANT WILDLIFE HABITAT   | 0 S.F.      |
| 8. ENDANGERED BOTANICAL RESOURCES | 0 S.F.      |

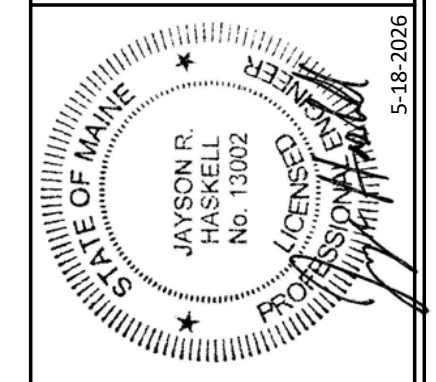
TOTAL NET AREA OF SUBDIVISION PARCEL: 378,271± S.F. (8.68± ACRES)

GENERAL NOTES:

1. THE OWNER OF RECORD OF THE PROPERTY IS GATEWAY DEVELOPMENT, LLC BY DEEDS RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK 24400 PAGE 96 AND BOOK 31719 PAGE 84.
2. TOTAL AREA OF THE PARCEL AREA IS APPROXIMATELY 12.23 ACRES.
3. PARCEL TAX MAP REFERENCE: TOWN OF WINDHAM ASSESSORS MAP 14 LOT 9-B.
4. PLAN REFERENCES:
  - A) BOUNDARY AND TOPOGRAPHIC SURVEY, TURNING LEAF DRIVE FOR BAKER BROOK FARM CREAMERY LLC AND GATEWAY DEVELOPMENT LLC, PREPARED BY SURVEY, INC.
  - B) HORIZONTAL DATUM: MAINE STATE PLANE, WEST ZONE, NAD83, U.S. FEET.
  - C) VERTICAL DATUM: NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88)
  - D) BOUNDARY SHOWN HEREON IS BASED ON PLAN REFERENCE 4A.
  - E) TOPOGRAPHIC CONTOURS SHOWN HEREON ARE BASED ON A COMBINATION OF ON-THE-GROUND SURVEY AND 2-FOOT LI DAR.
5. THE PROPERTY IS LOCATED IN THE COMMERCIAL - (C-1) ZONING DISTRICT WITH THE FOLLOWING DIMENSIONAL STANDARDS:

MIN LOT SIZE:	NONE
MIN STREET FRONTAGE:	100 FT
MAX FRONT YARD:	0 TO 20 FT
MIN SIDE/REAR YARD:	5 FT
MAX BUILDING HEIGHT:	75 FT
6. A PORTION OF THE SITE IS LOCATED WITHIN A ZONE AS FLOOD HAZARD AREA AS INDICATED ON THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) FLOOD INSURANCE RATE MAP (FIRM) PANEL 23005C0483F WITH AN EFFECTIVE DATE OF JUNE 20, 2024. NO DEVELOPMENT IS PROPOSED WITHIN THIS AREA.

PERMITTING PLAN  
NOT FOR  
CONSTRUCTION



**DM ROMA**  
CONSULTING ENGINEERS  
P.O. BOX 11116

REV	DATE	BY	DESCRIPTION
A	9-25-24	JRH	ISSUED FOR MDP PERMIT REVIEW
B	7-7-25	JRH	ISSUED FOR TOWN REVIEW
C	11-17-25	JRH	REVISED PER TOWN REVIEW COMMENTS
D	2-5-26	DMR	REVISED PER TOWN REVIEW COMMENTS
E	5-18-26	DMR	FINAL PLAN REVIEW

## OVERALL DEVELOPMENT PLAN

**TURNING LEAF HEIGHTS**  
TURNING LEAF DRIVE, DRIVE IN LANE & COMMONS AVENUE  
WINDHAM, MAINE

FOR RECORD OWNER:  
**GATEWAY DEVELOPMENT, LLC**  
4 COMMONS AVENUE - SUITE 12  
WINDHAM, ME 04062

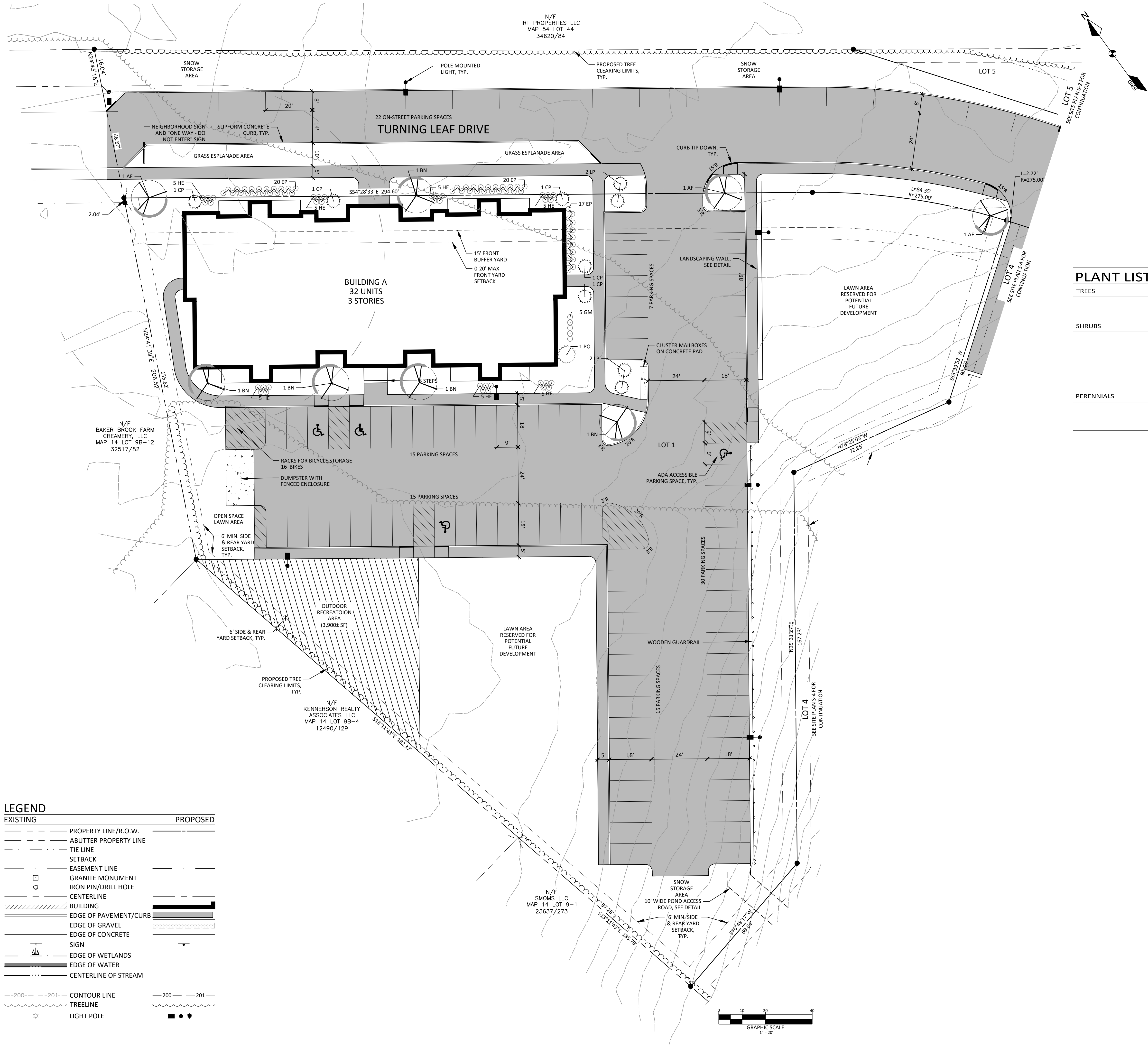
20013  
JOB NUMBER:

1" = 50'

5-18-2026

HEET 5 OF 19

OS-1



- GENERAL NOTES:**
- THE OWNER OF RECORD OF THE PROPERTY IS GATEWAY DEVELOPMENT, LLC BY DEEDS RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK 24400 PAGE 96 AND BOOK 31719 PAGE 84.
  - TOTAL AREA OF THE PARCEL IS APPROXIMATELY 12.33 ACRES. THE AREA OF LOT 1 IS APPROXIMATELY 76,548 SF.
  - PARCEL TAX MAP REFERENCE: TOWN OF WINDHAM ASSESSORS MAP 14 LOT 9-B.
  - PLAN REFERENCES:
    - A) BOUNDARY AND TOPOGRAPHIC SURVEY, TURNING LEAF DRIVE FOR BAKER BROOK FARM CREAMERY LLC AND GATEWAY DEVELOPMENT LLC, PREPARED BY SURVEY, INC.
    - B) SUBDIVISION PLAN OF TURNING LEAF HEIGHTS FOR GATEWAY DEVELOPMENT, LLC PREPARED BY DM ROMA CONSULTING ENGINEERS DATED _____ AND RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK _____ PAGE _____.
  - HORIZONTAL DATUM: MAINE STATE PLANE, WEST ZONE, NAD83, U.S. FEET.
  - VERTICAL DATUM: NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88)
  - BOUNDARY SHOWN HEREON IS BASED ON PLAN REFERENCE 4A.
  - TOPOGRAPHIC CONTOURS SHOWN HEREON ARE BASED ON A COMBINATION OF ON-THE-GROUND SURVEY AND 2-FOOT LIDAR.
  - THE PROPERTY IS LOCATED IN THE COMMERCIAL-1 (C-1) ZONING DISTRICT WITH THE FOLLOWING DIMENSIONAL STANDARDS:

MIN LOT SIZE:	NONE
MIN STREET FRONTAGE:	100 FT
MIN FRONT YARD:	0 TO 20 FT
MIN SIDE/REAR YARD:	6 FT
MAX BUILDING HEIGHT:	75 FT

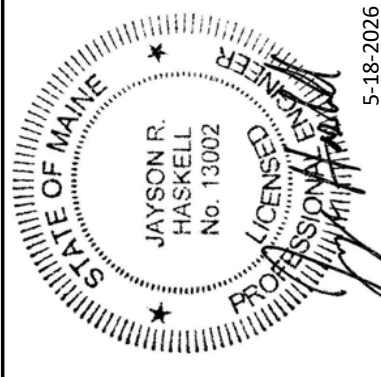
PLANT LIST					
TREES	QTY	KEY	BOTANICAL NAME	COMMON NAME	SIZE
	3	AF	Acer x freemanii 'Armstrong'	Armstrong Maple	3" CAL.
	5	BN	Betula nigra 'Heritage'	Heritage River Birch	10'-12'
SHRUBS	QTY	KEY	BOTANICAL NAME	COMMON NAME	SIZE
	5	CP	Cornus alba 'Argento-Marginata'	Variegated Red Twig Dogwood	6 GAL.
	0	IG	Ilex glabra 'Shamrock'	Compact Inkberry	10 GAL.
	1	PO	Physocarpus opulifolius 'Summer Wine'	Common Ninebark	2 GAL.
	4	LP	Syringa x hyacinthiflora 'Peachantosa'	Peachantosa Lilac	10 GAL.
	5	GM	Buxus 'Green Mountain'	Green Mountain Boxwood	6 GAL.
PERENNIALS	QTY	KEY	BOTANICAL NAME	COMMON NAME	SIZE
	30	HE	Hemerocallis 'Happy Returns'	Yellow Daylily	2 GAL.
	57	EP	Echinacea purpurea 'Kim's Knee High'	Kim's Knee High Cone Flower	1 GAL.

LEGEND	
EXISTING	PROPOSED
	PROPERTY LINE/R.O.W.
	ABUTTER PROPERTY LINE
	TIE LINE
	SETBACK
	EASEMENT LINE
	GRANITE MONUMENT
	IRON PIN/DRILL HOLE
	CENTERLINE
	BUILDING
	EDGE OF PAVEMENT/CURB
	EDGE OF GRAVEL
	EDGE OF CONCRETE
	SIGN
	EDGE OF WETLANDS
	EDGE OF WATER
	CENTERLINE OF STREAM
	CONTOUR LINE
	TREELINE
	LIGHT POLE

APPROVED - WINDHAM PLANNING BOARD:

CHAIRPERSON	DATE

PERMITTING PLAN  
NOT FOR  
CONSTRUCTION



**DM ROMA**  
CONSULTING ENGINEERS  
P.O. BOX 1116  
WINDHAM, ME 04062  
(207) 591-5055

REV	DATE	BY	DESCRIPTION
A	9-25-24	JRH	ISSUED FOR MSEP PERMIT REVIEW
B	7-7-25	JRH	ISSUED FOR TOWN REVIEW
C	11-17-25	JRH	REVISED PER TOWN REVIEW COMMENTS
D	2-5-26	DMR	REVISED PER TOWN REVIEW COMMENTS
E	5-18-26	DMR	FINAL PLAN REVIEW

**SITE & LANDSCAPING PLAN - LOT 1**  
**TURNING LEAF HEIGHTS**  
TURNING LEAF DRIVE, DRIVE IN LANE & COMMONS AVENUE  
WINDHAM, MAINE  
FOR RECORD OWNER:  
**GATEWAY DEVELOPMENT, LLC**  
4 COMMONS AVENUE - SUITE 12  
WINDHAM, ME 04062

20013 JOB NUMBER:
1" = 20' SCALE:
5-18-2026 DATE:
SHEET 6 OF 19
S-1

GENERAL NOTES:

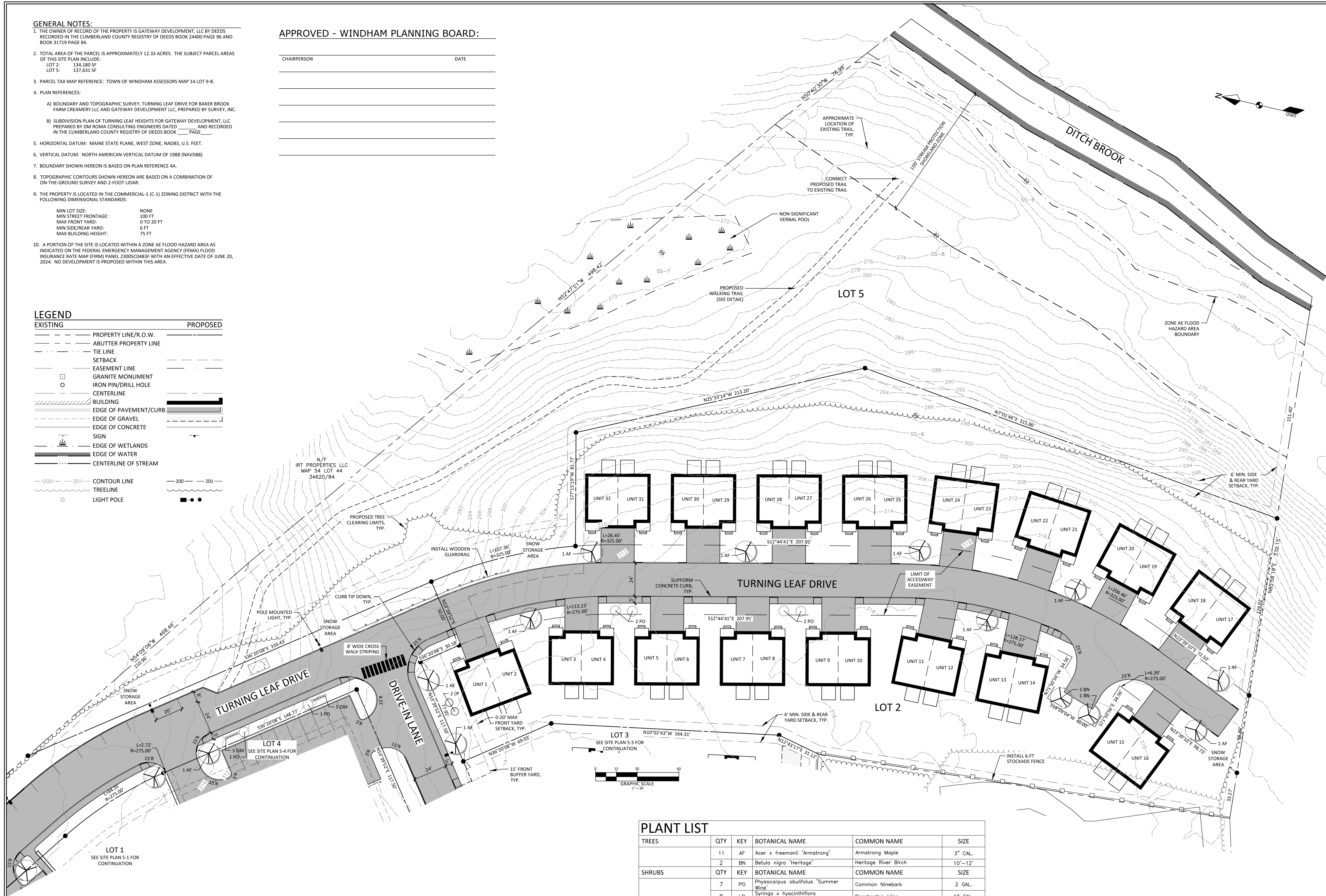
1. THE OWNER OF RECORD OF THE PROPERTY IS GATEWAY DEVELOPMENT, LLC BY DEEDS RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK 24400 PAGE 96 AND BOOK 31719 PAGE 84.
2. TOTAL AREA OF THE PARCEL IS APPROXIMATELY 12.33 ACRES. THE SUBJECT PARCEL AREAS OF THIS SITE PLAN INCLUDE:  
LOT 2: 134,180 SF  
LOT 5: 137,631 SF
3. PARCEL TAX MAP REFERENCE: TOWN OF WINDHAM ASSESSORS MAP 14 LOT 9-B.
4. PLAN REFERENCES:  
A) BOUNDARY AND TOPOGRAPHIC SURVEY, TURNING LEAF DRIVE FOR BAKER BROOK FARM CREAMERY LLC AND GATEWAY DEVELOPMENT LLC, PREPARED BY SURVEY, INC.  
B) SUBDIVISION PLAN OF TURNING LEAF HEIGHTS FOR GATEWAY DEVELOPMENT, LLC PREPARED BY DM ROMA CONSULTING ENGINEERS DATED _____ AND RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK _____ PAGE _____
5. HORIZONTAL DATUM: MAINE STATE PLANE, WEST ZONE, NAD83, U.S. FEET.
6. VERTICAL DATUM: NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88)
7. BOUNDARY SHOWN HEREON IS BASED ON PLAN REFERENCE 4A.
8. TOPOGRAPHIC CONTOURS SHOWN HEREON ARE BASED ON A COMBINATION OF ON-THE-GROUND SURVEY AND 2-FOOT LIDAR.
9. THE PROPERTY IS LOCATED IN THE COMMERCIAL-1 (C-1) ZONING DISTRICT WITH THE FOLLOWING DIMENSIONAL STANDARDS:
- |                      |            |
|----------------------|------------|
| MIN LOT SIZE:        | NONE       |
| MIN STREET FRONTAGE: | 100 FT     |
| MAX FRONT YARD:      | 0 TO 20 FT |
| MIN SIDE/REAR YARD:  | 6 FT       |
| MAX BUILDING HEIGHT: | 75 FT      |
10. A PORTION OF THE SITE IS LOCATED WITHIN A ZONE AS FLOOD HAZARD AREA AS INDICATED ON THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) FLOOD INSURANCE RATE MAP (FIRM) PANEL 23005C0483F WITH AN EFFECTIVE DATE OF JUNE 20, 2024. NO DEVELOPMENT IS PROPOSED WITHIN THIS AREA.

LEGEND

EXISTING	PROPOSED
PROPERTY LINE/R.O.W.	PROPERTY LINE/R.O.W.
ABUTTER PROPERTY LINE	ABUTTER PROPERTY LINE
TIE LINE	TIE LINE
SETBACK	SETBACK
EASEMENT LINE	EASEMENT LINE
GRANITE MONUMENT	GRANITE MONUMENT
IRON PIN/DRILL HOLE	IRON PIN/DRILL HOLE
CENTERLINE	CENTERLINE
BUILDING	BUILDING
EDGE OF PAVEMENT/CURB	EDGE OF PAVEMENT/CURB
EDGE OF GRAVEL	EDGE OF GRAVEL
EDGE OF CONCRETE	EDGE OF CONCRETE
SIGN	SIGN
EDGE OF WETLANDS	EDGE OF WETLANDS
EDGE OF WATER	EDGE OF WATER
CENTERLINE OF STREAM	CENTERLINE OF STREAM
CONTOUR LINE	CONTOUR LINE
TREELINE	TREELINE
LIGHT POLE	LIGHT POLE

APPROVED - WINDHAM PLANNING BOARD:

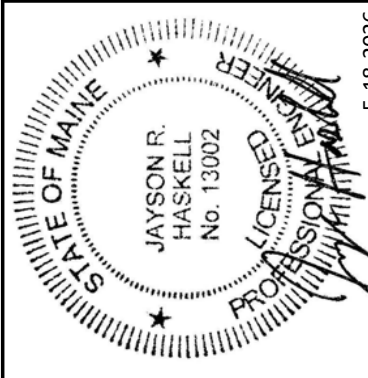
CHAIRPERSON	DATE



PLANT LIST

TREES	QTY	KEY	BOTANICAL NAME	COMMON NAME	SIZE
	11	AF	Acer x freemanii 'Armstrong'	Armstrong Maple	3" CAL.
	2	BN	Betula nigra 'Heritage'	Heritage River Birch	10"-12"
SHRUBS	QTY	KEY	BOTANICAL NAME	COMMON NAME	SIZE
	7	PO	Physocarpus obulifolus 'Summer Wine'	Common Ninebark	2 GAL.
	8	LP	Syringa x hyacinthiflora 'Pocahontas'	Pocahontas Lilac	10 GAL.
	10	GM	Buxus 'Green Mountain'	Green Mountain Boxwood	6 GAL.

PERMITTING PLAN  
NOT FOR  
CONSTRUCTION



**DM ROMA**  
CONSULTING ENGINEERS  
P.O. BOX 1116  
WINDHAM, ME 04062  
(207) 591-5055

REV	DATE	BY	DESCRIPTION
A	9-25-24	JRH	ISSUED FOR MSEP PERMIT REVIEW
B	7-7-25	JRH	ISSUED FOR TOWN REVIEW
C	11-17-25	JRH	REVISED PER TOWN REVIEW COMMENTS
D	2-5-26	DMR	REVISED PER TOWN REVIEW COMMENTS
E	5-18-26	DMR	FINAL PLAN REVIEW

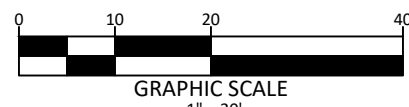
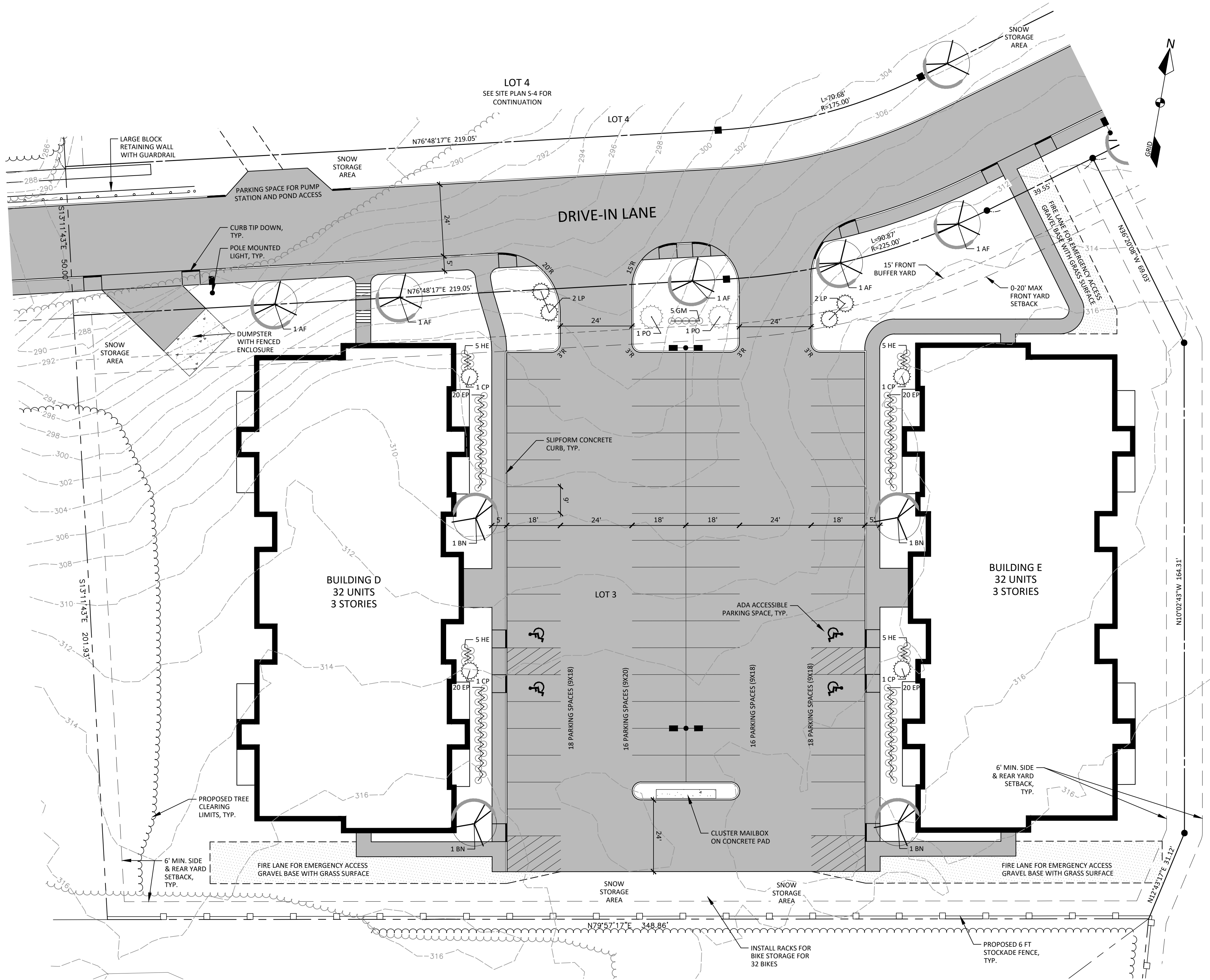
SITE & LANDSCAPING PLAN - LOTS 2 & 5

TURNING LEAF HEIGHTS  
TURNING LEAF DRIVE, DRIVE IN LANE & COMMONS AVENUE  
WINDHAM, MAINE  
FOR RECORD OWNER:  
**GATEWAY DEVELOPMENT, LLC**  
4 COMMONS AVENUE, SUITE 12  
WINDHAM, ME 04062

20013 JOB NUMBER:
1" = 30' SCALE:
5-18-2026 DATE:
SHEET 7 OF 19
S-2

LEGEND

EXISTING	PROPOSED
	PROPERTY LINE/R.O.W.
	ABUTTER PROPERTY LINE
	TIE LINE
	SETBACK
	EASEMENT LINE
	GRANITE MONUMENT
	IRON PIN/DRILL HOLE
	CENTERLINE
	BUILDING
	EDGE OF PAVEMENT/CURB
	EDGE OF GRAVEL
	EDGE OF CONCRETE
	SIGN
	EDGE OF WETLANDS
	EDGE OF WATER
	CENTERLINE OF STREAM
	CONTOUR LINE
	TREELINE
	LIGHT POLE



PLANT LIST

TREES	QTY	KEY	BOTANICAL NAME	COMMON NAME	SIZE
	5	AF	Acer x freemanii 'Armstrong'	Armstrong Maple	3" CAL.
	4	BN	Betula nigra 'Heritage'	Heritage River Birch	10'-12'
SHRUBS	QTY	KEY	BOTANICAL NAME	COMMON NAME	SIZE
	4	CP	Cornus alba 'Argento-Marginata'	Variegated Red Twig Dogwood	6 GAL.
	2	PO	Physocarpus opulifolius 'Summer Wine'	Common Ninebark	2 GAL.
	4	LP	Syringa x hyacinthiflora 'Pocahontas'	Pocahontas Lilac	10 GAL.
	5	GM	Buxus 'Green Mountain'	Green Mountain Boxwood	6 GAL.
PERENNIALS	QTY	KEY	BOTANICAL NAME	COMMON NAME	SIZE
	20	HE	Hemerocallis 'Happy Returns'	Yellow Daylily	2 GAL.
	80	EP	Echinacea purpurea 'Kim's Knee High'	Kim's Knee High Cone Flower	1 GAL.

GENERAL NOTES:

- THE OWNER OF RECORD OF THE PROPERTY IS GATEWAY DEVELOPMENT, LLC BY DEEDS RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK 24400 PAGE 96 AND BOOK 31719 PAGE 84.
- TOTAL AREA OF THE PARCEL IS APPROXIMATELY 12.33 ACRES. THE AREA OF LOT 3 IS APPROXIMATELY 79,305 SF.
- PARCEL TAX MAP REFERENCE: TOWN OF WINDHAM ASSESSORS MAP 14 LOT 9-B.
- PLAN REFERENCES:
  - BOUNDARY AND TOPOGRAPHIC SURVEY, TURNING LEAF DRIVE FOR BAKER BROOK FARM CREAMERY LLC AND GATEWAY DEVELOPMENT LLC, PREPARED BY SURVEY, INC.
  - SUBDIVISION PLAN OF TURNING LEAF HEIGHTS FOR GATEWAY DEVELOPMENT, LLC PREPARED BY DM ROMA CONSULTING ENGINEERS DATED _____ AND RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK _____ PAGE _____
- HORIZONTAL DATUM: MAINE STATE PLANE, WEST ZONE, NAD83, U.S. FEET.
- VERTICAL DATUM: NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88)
- BOUNDARY SHOWN HEREON IS BASED ON PLAN REFERENCE 4A.
- TOPOGRAPHIC CONTOURS SHOWN HEREON ARE BASED ON A COMBINATION OF ON-THE-GROUND SURVEY AND 2-FOOT LIDAR.
- THE PROPERTY IS LOCATED IN THE COMMERCIAL-1 (C-1) ZONING DISTRICT WITH THE FOLLOWING DIMENSIONAL STANDARDS:

MIN LOT SIZE:	NONE
MIN STREET FRONTAGE:	100 FT
MAX FRONT YARD:	0 TO 20 FT
MIN SIDE/REAR YARD:	6 FT
MAX BUILDING HEIGHT:	75 FT

APPROVED - WINDHAM PLANNING BOARD:

CHAIRPERSON	DATE

SITE & LANDSCAPING PLAN - LOT 3

TURNING LEAF HEIGHTS  
TURNING LEAF DRIVE, DRIVE IN LANE & COMMONS AVENUE  
WINDHAM, MAINE  
FOR RECORD OWNER:  
GATEWAY DEVELOPMENT, LLC  
4 COMMONS AVENUE - SUITE 12  
WINDHAM, ME 04092

20013  
JOB NUMBER:

1" = 20'

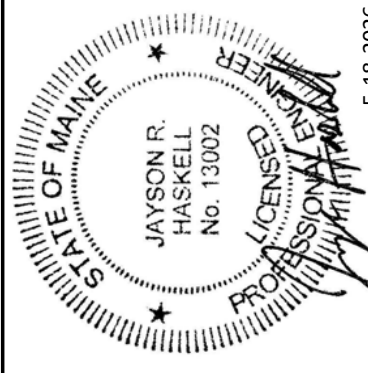
5-18-2026

DATE:

SHEET 8 OF 19

S-3

PERMITTING PLAN  
NOT FOR  
CONSTRUCTION

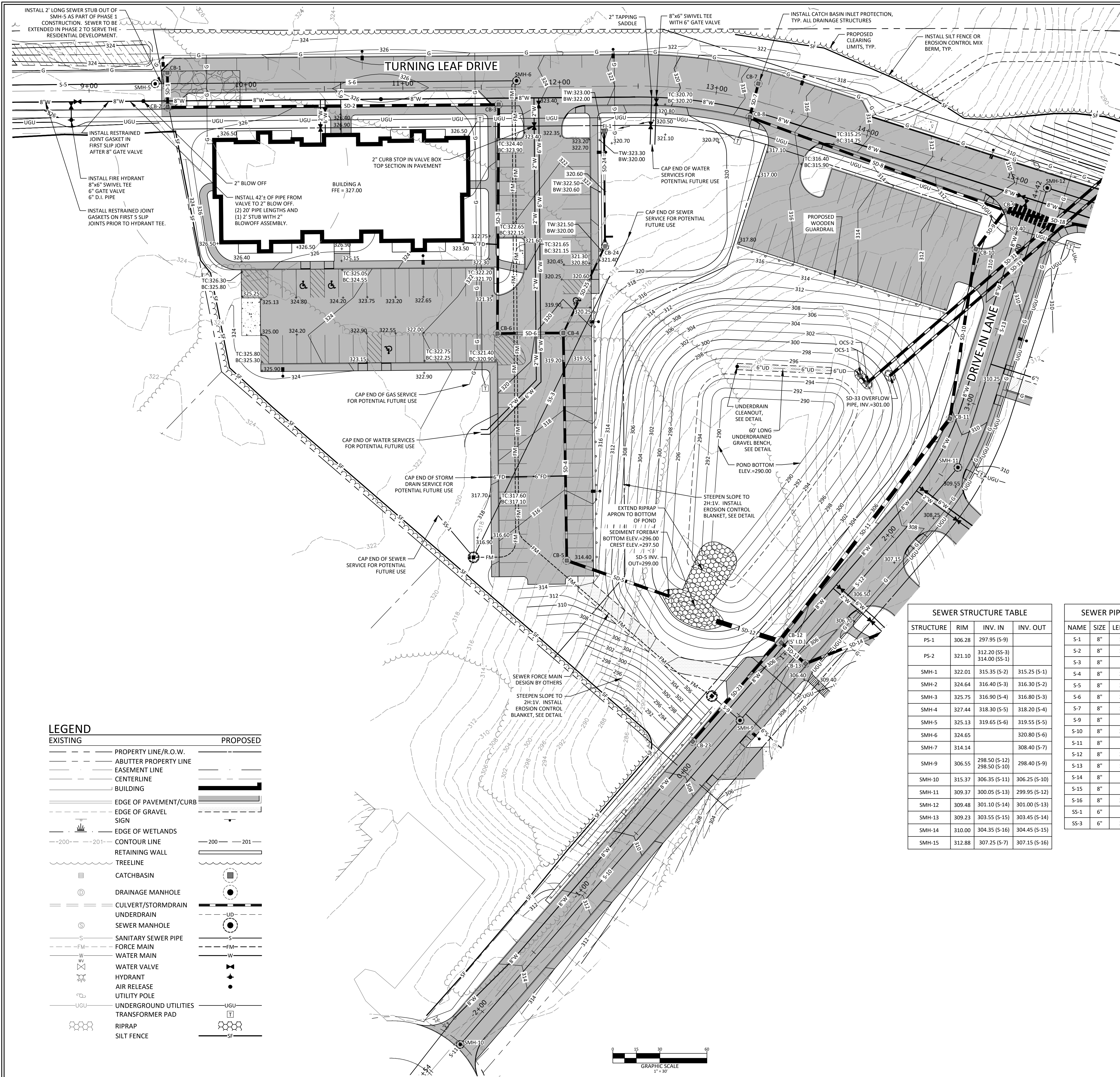


**DM ROMA**  
CONSULTING ENGINEERS

P.O. BOX 1116  
WINDHAM, ME 04092  
(207) 591-5055

REV	DATE	BY	DESCRIPTION
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E	5-18-26	DMR	FINAL PLAN REVIEW





LEGEND	
EXISTING	PROPOSED

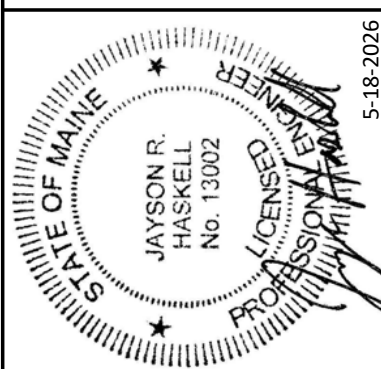
SEWER STRUCTURE TABLE			
STRUCTURE	RIM	INV. IN	INV. OUT
PS-1	306.28	297.95 (S-9)	
PS-2	321.10	312.20 (SS-3)	
		314.00 (SS-3)	
SMH-1	322.01	315.35 (S-2)	315.25 (S-1)
SMH-2	324.64	316.40 (S-3)	316.30 (S-2)
SMH-3	325.75	316.90 (S-4)	316.80 (S-3)
SMH-4	327.44	318.30 (S-5)	318.20 (S-4)
SMH-5	325.13	319.65 (S-6)	319.55 (S-5)
SMH-6	324.65		320.80 (S-6)
SMH-7	314.14		308.40 (S-7)
SMH-9	306.55	298.50 (S-12)	298.40 (S-9)
		298.50 (S-10)	
SMH-10	315.37	306.35 (S-11)	306.25 (S-10)
SMH-11	309.37	300.05 (S-13)	299.95 (S-12)
SMH-12	309.48	301.10 (S-14)	301.00 (S-13)
SMH-13	309.23	303.55 (S-15)	303.45 (S-14)
SMH-14	310.00	304.35 (S-16)	304.45 (S-15)
SMH-15	312.88	307.25 (S-7)	307.15 (S-16)

SEWER PIPE TABLE			
NAME	SIZE	LENGTH	SLOPE
S-1	8"	172'	0.53%
S-2	8"	188'	0.52%
S-3	8"	82'	0.51%
S-4	8"	258'	0.51%
S-5	8"	249'	0.51%
S-6	8"	230'	0.51%
S-7	8"	130'	0.91%
S-9	8"	24'	2.41%
S-10	8"	273'	2.88%
S-11	8"	7'	1.38%
S-12	8"	213'	0.69%
S-13	8"	187'	0.52%
S-14	8"	78'	3.16%
S-15	8"	89'	1.05%
S-16	8"	290'	0.98%
SS-1	6"	35'	6.90%
SS-3	6"	226'	2.02%

STORM DRAIN STRUCTURE TABLE			
STRUCTURE	RIM	INV. IN	INV. OUT
CB-1	324.90		321.15 (SD-1)
CB-2	324.90	320.95 (SD-1)	320.50 (SD-2)
CB-3	324.87	319.20 (SD-2)	319.10 (SD-3)
CB-4	319.57	314.17 (SD-25)	313.50 (SD-4)
		313.75 (SD-6)	
CB-5	314.20	307.00 (SD-4)	306.00 (SD-5)
CB-6	320.60	316.35 (SD-3)	316.25 (SD-6)
CB-7	317.42		313.20 (SD-7)
CB-8	317.42	313.00 (SD-7)	312.90 (SD-8)
CB-9	309.20	304.40 (SD-8)	304.05 (SD-9)
		304.15 (SD-18)	
CB-10	310.35	303.90 (SD-9)	303.80 (SD-10)
CB-11	309.83	303.25 (SD-10)	303.15 (SD-11)
CB-12	305.78	301.10 (SD-11)	301.00 (SD-12)
		301.10 (SD-13)	
		301.35 (SD-23)	
CB-13	305.78	301.35 (SD-14)	301.25 (SD-13)
CB-14	307.25	302.00 (SD-16)	301.75 (SD-14)
		302.00 (SD-15)	
CB-15	304.79	302.70 (SD-29)	302.60 (SD-15)
CB-16	310.00	305.35 (SD-17)	305.25 (SD-16)
CB-17	310.00		305.75 (SD-17)
CB-18	308.83	304.62 (SD-20)	304.52 (SD-18)
		304.62 (SD-19)	
CB-19	308.83		304.82 (SD-19)
CB-20	309.96	305.75 (SD-21)	305.65 (SD-20)
CB-21	311.45	307.55 (SD-22)	307.45 (SD-21)
CB-22	311.45		307.75 (SD-22)
CB-23	307.03		302.75 (SD-23)
CB-24	319.90	315.00 (SD-24)	314.90 (SD-25)
CB-25	324.80	320.95 (SD-26)	320.85 (SD-27)
DMH-1	309.00	301.90 (SD-36)	301.80 (SD-37)
FI-1	319.90		315.90 (SD-24)
FI-2	308.40		305.10 (SD-28)
FI-3	307.20	303.60 (SD-28)	303.50 (SD-29)
OCS-1	298.00	293.00 (SD-32)	292.90 (SD-30)
OCS-2	298.00	292.70 (SD-30)	292.70 (SD-31)

STORM DRAIN PIPE TABLE			
NAME	SIZE	LENGTH	SLOPE
SD-1	15"	22'	1.11%
SD-2	15"	211'	0.63%
SD-3	15"	146'	1.94%
SD-4	15"	145'	4.61%
SD-5	18"	69'	10.38%
SD-6	15"	42'	6.56%
SD-7	15"	22'	1.11%
SD-8	15"	173'	5.02%
SD-9	18"	31'	0.56%
SD-10	18"	109'	0.52%
SD-11	24"	179'	1.18%
SD-12	30"	42'	5.03%
SD-13	18"	22'	0.86%
SD-14	18"	69'	0.62%
SD-15	15"	82'	0.77%
SD-16	15"	177'	1.88%
SD-17	15"	82'	0.51%
SD-18	18"	68'	0.58%
SD-19	15"	22'	1.11%
SD-20	15"	121'	0.88%
SD-21	15"	149'	1.18%
SD-22	15"	22'	1.11%
SD-23	15"	85'	1.75%
SD-24	10"	73'	1.28%
SD-25	10"	61'	1.28%
SD-26	15"	35'	0.90%
SD-27	15"	30'	3.58%
SD-28	10"	181'	0.84%
SD-29	10"	124'	0.66%
SD-30	12"	6'	4.64%
SD-31	15"	242'	1.12%
SD-32	6"	3'	0.00%
SD-33	15"	230'	4.78%
SD-34	6"	98'	1.78%
SD-35	6"	188'	0.93%
SD-36	10"	175'	0.98%
SD-37	10"	125'	1.47%

PERMITTING PLAN  
NOT FOR  
CONSTRUCTION



**DM ROMA**  
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REV	DATE	BY	DESCRIPTION
A	9-25-24	JRH	ISSUED FOR MSEP PERMIT REVIEW
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C	11-17-25	JRH	REVISED PER TOWN REVIEW COMMENTS
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E	5-18-26	JRH	FINAL PLAN REVIEW

GRADING AND UTILITY PLAN

TURNING LEAF HEIGHTS

TURNING LEAF DRIVE AND DRIVE-IN LANE  
WINDHAM, MAINE  
FOR: GATEWAY DEVELOPMENT, LLC  
4 COMMANDERS AVENUE - SUITE 12  
WINDHAM, ME 04062

20013  
JOB NUMBER:  
1" = 30'  
SCALE:  
5-18-2026  
DATE:  
SHEET 10 OF 19  
GU-1

LEGEND

EXISTING	PROPOSED	EXISTING	PROPOSED
PROPERTY LINE/R.O.W.		DRAINAGE MANHOLE	
ABUTTER PROPERTY LINE		CULVERT/STORMDRAIN	
EASEMENT LINE		UNDERDRAIN	
CENTERLINE		SEWER MANHOLE	
BUILDING		SANITARY SEWER PIPE	
EDGE OF PAVEMENT/CURB		FORCE MAIN	
EDGE OF GRAVEL		WATER MAIN	
SIGN		WATER VALVE	
EDGE OF WETLANDS		HYDRANT	
CONTOUR LINE		AIR RELEASE	
RETAINING WALL		UTILITY POLE	
TREELINE		UNDERGROUND UTILITIES	
CATCHBASIN		TRANSFORMER PAD	
		RIPRAP	
		SILT FENCE	

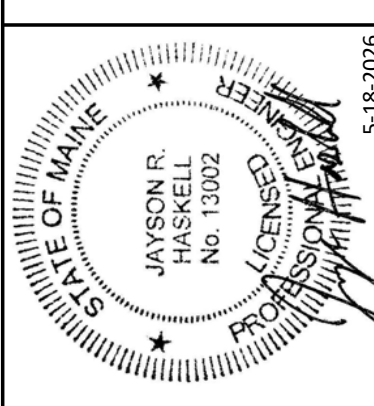
STORM DRAIN STRUCTURE TABLE			
STRUCTURE	RIM	INV. IN	INV. OUT
CB-1	324.90		321.15 (SD-1)
CB-2	324.90	320.95 (SD-1)	320.50 (SD-2)
CB-3	324.87	319.20 (SD-2)	319.10 (SD-3)
CB-4	319.57	314.17 (SD-25) 313.75 (SD-6)	313.50 (SD-4)
CB-5	314.20	307.00 (SD-4)	306.00 (SD-5)
CB-6	320.60	316.35 (SD-3)	316.25 (SD-6)
CB-7	317.42		313.20 (SD-7)
CB-8	317.42	313.00 (SD-7)	312.90 (SD-8)
CB-9	309.20	304.40 (SD-8) 304.15 (SD-18)	304.05 (SD-9)
CB-10	310.35	303.90 (SD-9)	303.80 (SD-10)
CB-11	309.83	303.25 (SD-10)	303.15 (SD-11)
CB-12	305.78	301.10 (SD-11) 301.10 (SD-13) 301.35 (SD-23)	301.00 (SD-12)
CB-13	305.78	301.35 (SD-14)	301.25 (SD-13)
CB-14	307.25	302.00 (SD-16) 302.00 (SD-15)	301.75 (SD-14)
CB-15	304.79	302.70 (SD-29)	302.60 (SD-15)
CB-16	310.00	305.35 (SD-17)	305.25 (SD-16)
CB-17	310.00		305.75 (SD-17)
CB-18	308.83	304.62 (SD-20) 304.62 (SD-19)	304.52 (SD-18)
CB-19	308.83		304.82 (SD-19)
CB-20	309.96	305.75 (SD-21)	305.65 (SD-20)
CB-21	311.45	307.55 (SD-22)	307.45 (SD-21)
CB-22	311.45		307.75 (SD-22)
CB-23	307.03		302.75 (SD-23)
CB-24	319.90	315.00 (SD-24)	314.90 (SD-25)
CB-25	324.80	320.95 (SD-26)	320.85 (SD-27)
DMH-1	309.00	301.90 (SD-36)	301.80 (SD-37)
FI-1	319.90		315.90 (SD-24)
FI-2	308.40		305.10 (SD-28)
FI-3	307.20	303.60 (SD-28)	303.50 (SD-29)
OCS-1	298.00	293.00 (SD-32)	292.90 (SD-30)
OCS-2	298.00	292.70 (SD-30)	292.70 (SD-31)

STORM DRAIN PIPE TABLE			
NAME	SIZE	LENGTH	SLOPE
SD-1	15"	22'	1.11%
SD-2	15"	211'	0.63%
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SD-4	15"	145'	4.61%
SD-5	18"	69'	10.38%
SD-6	15"	42'	6.56%
SD-7	15"	22'	1.11%
SD-8	15"	173'	5.02%
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SD-22	15"	22'	1.11%
SD-23	15"	85'	1.75%
SD-24	10"	73'	1.28%
SD-25	10"	61'	1.28%
SD-26	15"	35'	0.90%
SD-27	15"	30'	3.58%
SD-28	10"	181'	0.84%
SD-29	10"	124'	0.66%
SD-30	12"	6'	4.64%
SD-31	15"	242'	1.12%
SD-32	6"	3'	0.00%
SD-33	15"	230'	4.78%
SD-34	6"	98'	1.78%
SD-35	6"	188'	0.93%
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SEWER STRUCTURE TABLE			
STRUCTURE	RIM	INV. IN	INV. OUT
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SMH-5	325.13	319.65 (S-6)	319.55 (S-5)
SMH-6	324.65		320.80 (S-6)
SMH-7	314.14		308.40 (S-7)
SMH-9	306.55	298.50 (S-12) 298.50 (S-10)	298.40 (S-9)
SMH-10	315.37	306.35 (S-11)	306.25 (S-10)
SMH-11	309.37	300.05 (S-13)	299.95 (S-12)
SMH-12	309.48	301.10 (S-14)	301.00 (S-13)
SMH-13	309.23	303.55 (S-15)	303.45 (S-14)
SMH-14	310.00	304.35 (S-16)	304.45 (S-15)
SMH-15	312.88	307.25 (S-7)	307.15 (S-16)

SEWER PIPE TABLE			
NAME	SIZE	LENGTH	SLOPE
S-1	8"	172'	0.53%
S-2	8"	188'	0.52%
S-3	8"	82'	0.51%
S-4	8"	258'	0.51%
S-5	8"	249'	0.51%
S-6	8"	230'	0.51%
S-7	8"	130'	0.91%
S-9	8"	24'	2.41%
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S-13	8"	187'	0.52%
S-14	8"	78'	3.16%
S-15	8"	89'	1.05%
S-16	8"	290'	0.98%
SS-1	6"	35'	6.90%
SS-3	6"	226'	2.02%

PERMITTING PLAN  
NOT FOR  
CONSTRUCTION



**DM ROMA**  
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WINDHAM, ME 04062  
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C	11-17-25	JRH	REVISED PER TOWN REVIEW COMMENTS
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**GRADING AND UTILITY PLAN**  
TURNING LEAF HEIGHTS  
TURNING LEAF DRIVE AND DRIVE-IN LANE  
WINDHAM, MAINE  
FOR:  
GATEWAY DEVELOPMENT, LLC  
4 CROMWELL AVENUE SUITE 12  
WINDHAM, ME 04062

20013
JOB NUMBER:
1" = 30'
SCALE:
5-18-2026
DATE:
SHEET 11 OF 19
GU-2

# LEGEND

EXISTING	PROPOSED	EXISTING	PROPOSED

ROOSEVELT TRAIL  
(ROUTE 302)

SEWER INSTALLED  
BY OTHERS

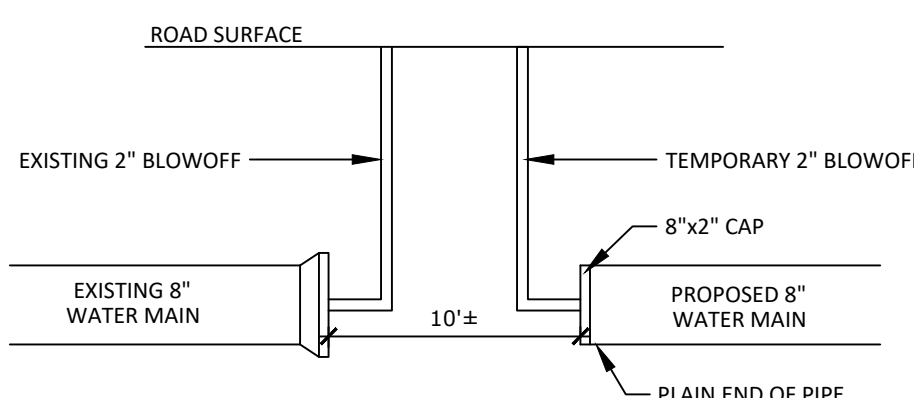
SMH INSTALLED BY OTHERS

CONNECT INTO SEWER STUB  
TARGET INVERT AT STRUCTURE =314.35

TURNING LEAF DRIVE

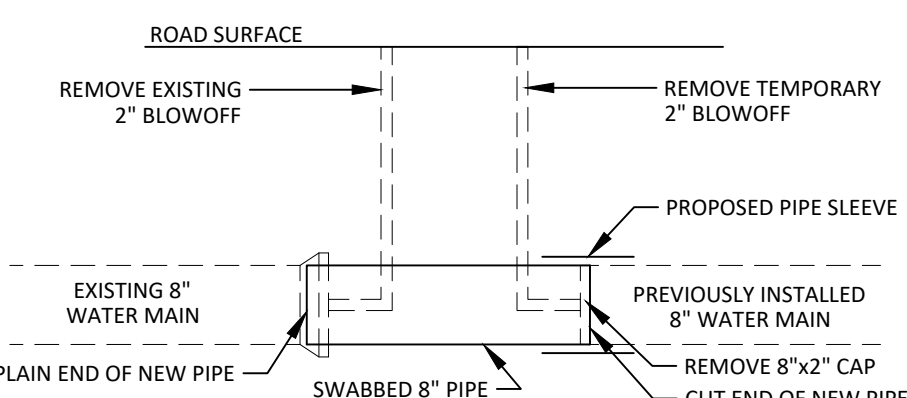
ROAD PLAN VIEW: TURNING LEAF DRIVE

SCALE: 1"=30'



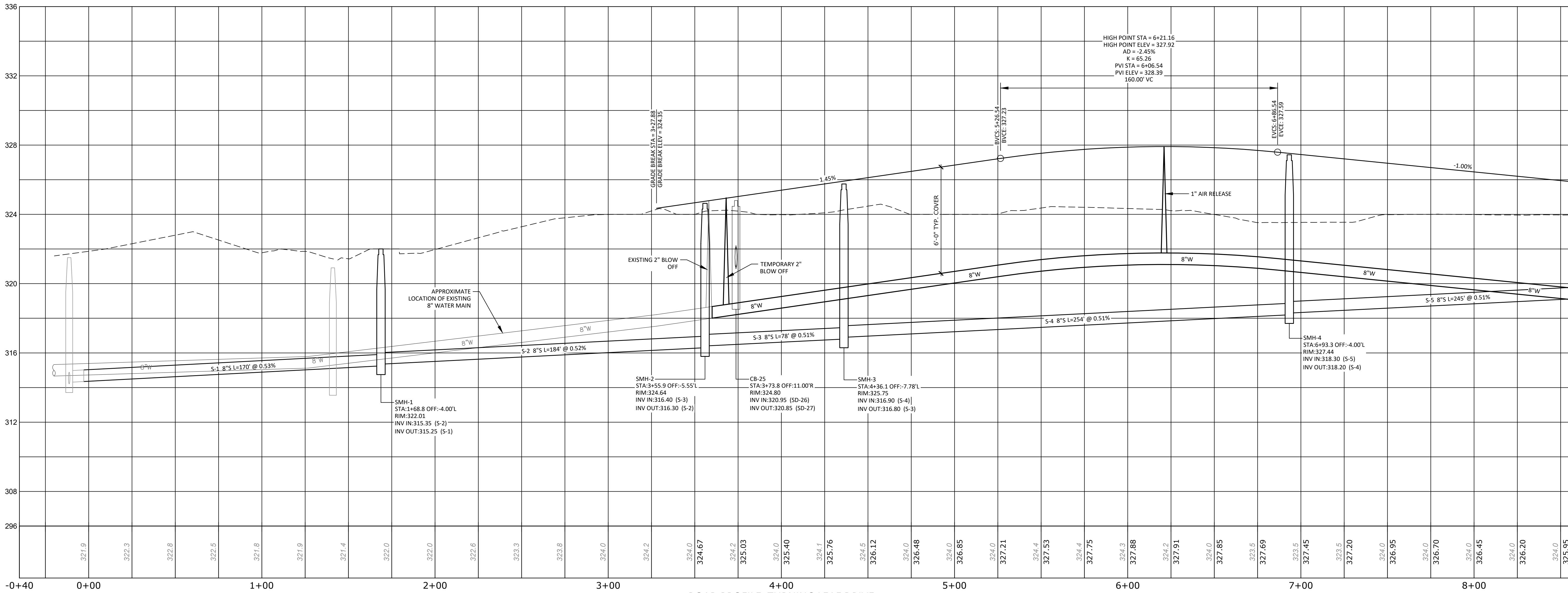
TEMPORARY WATER MAIN INSTALLATION

NOT TO SCALE



FINAL WATER MAIN CONNECTION

NOT TO SCALE

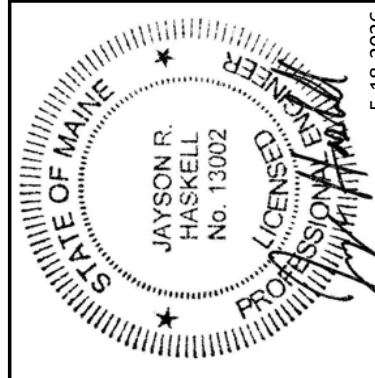


ROAD PROFILE: TURNING LEAF DRIVE

SCALE: HORIZ.: 1"=30'  
VERT.: 1"=3'



PERMITTING PLAN  
NOT FOR  
CONSTRUCTION



**DM ROMA**  
CONSULTING ENGINEERS

P.O. BOX 1116  
WINDHAM, ME 04062  
(207) 591-5055

ROADWAY PLAN & PROFILE: TURNING LEAF DRIVE

TURNING LEAF HEIGHTS  
TURNING LEAF DRIVE, DRIVE IN LANE & COMMONS AVENUE  
WINDHAM, MAINE  
FOR RECORD OWNER:  
**GATEWAY DEVELOPMENT, LLC**  
4 COMMONS AVENUE, SUITE 12  
WINDHAM, ME 04062

20013  
JOB NUMBER:

1" = 30'  
SCALE:

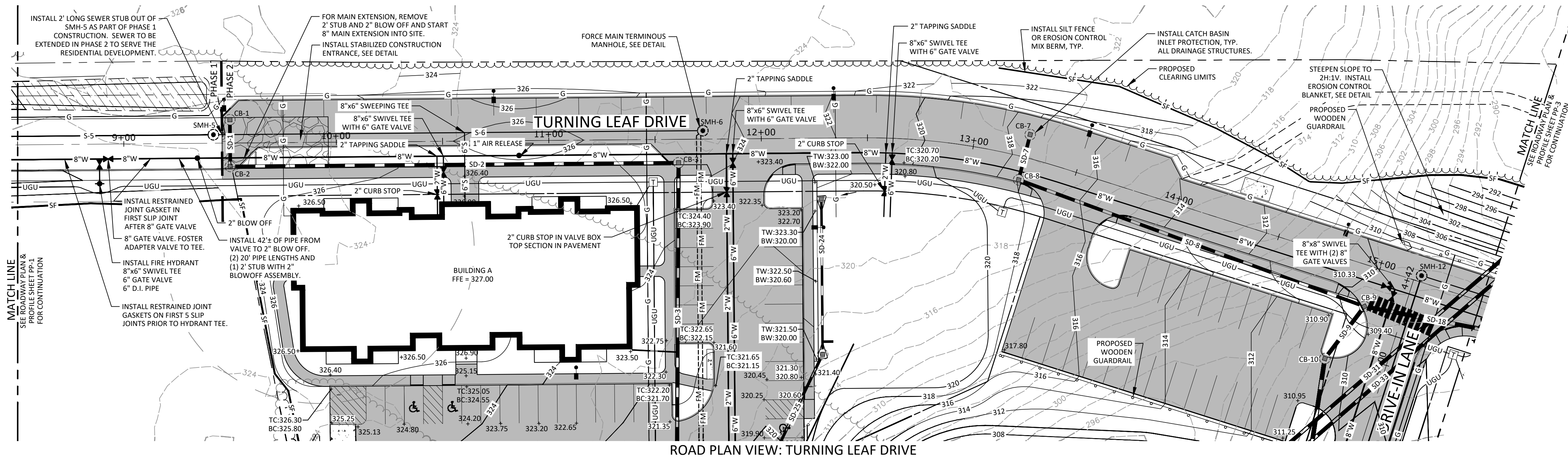
5-18-2026  
DATE:

SHEET 12 OF 19

PP-1

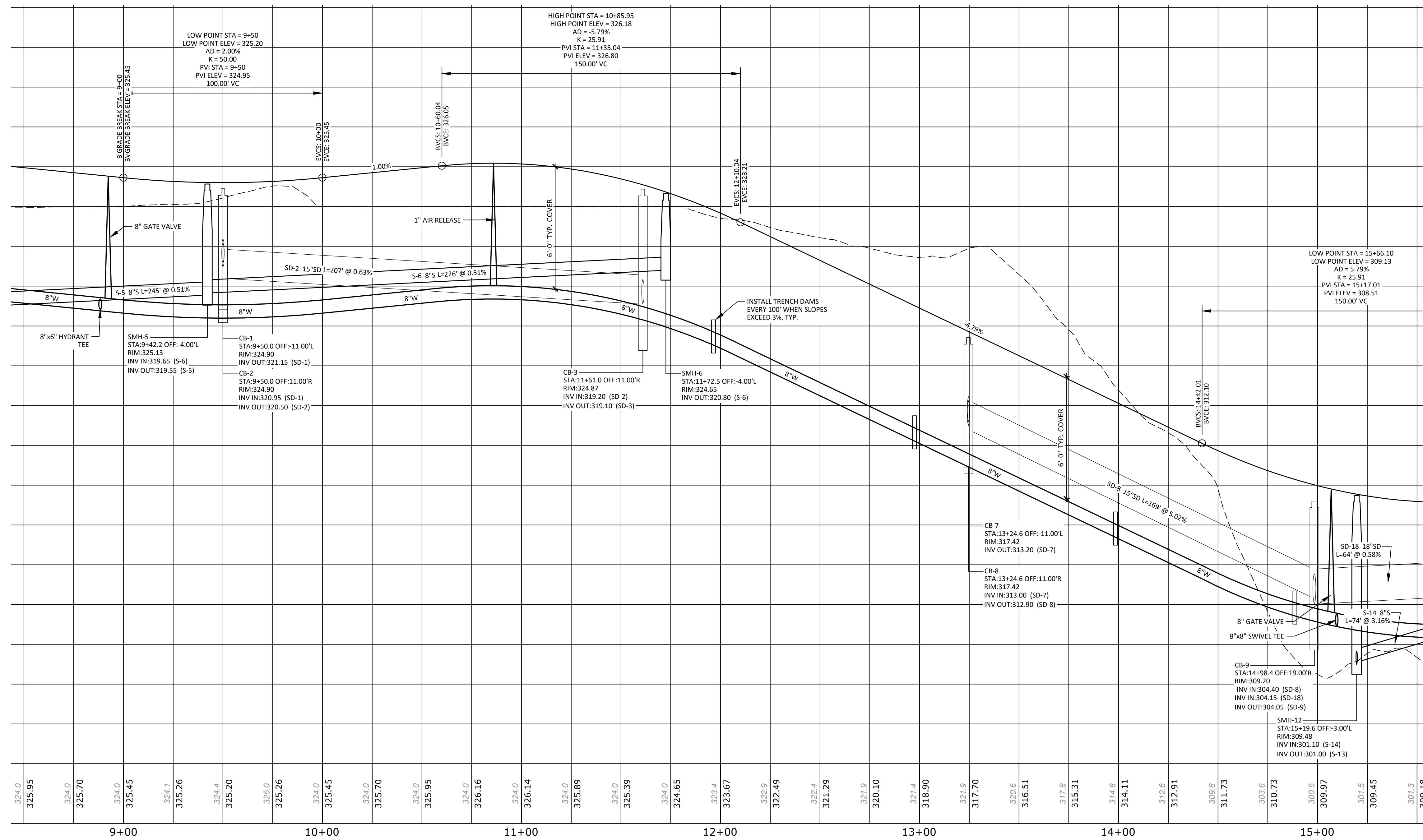
## CONSTRUCTION NOTES:

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- VERTICAL DATUM: NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88). TOPOGRAPHIC FEATURES SHOWN HEREON ARE BASED ON THE GROUND SURVEY AND 2-FOOT LIDAR OBTAINED FROM THE MAINE OFFICE OF GIS.
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- ALL WATER MAIN PIPE SHALL BE 8" DIA. CLASS 52 DUCTILE IRON PIPE OR APPROVED ALTERNATIVE BY THE PORTLAND WATER DISTRICT. CONTRACTOR/OWNER SHALL COORDINATE WITH THE PORTLAND WATER DISTRICT PRIOR TO ORDERING MATERIALS AND INSTALLING WATER INFRASTRUCTURE.
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ROAD PLAN VIEW: TURNING LEAF DRIVE

SCALE: 1"=30'

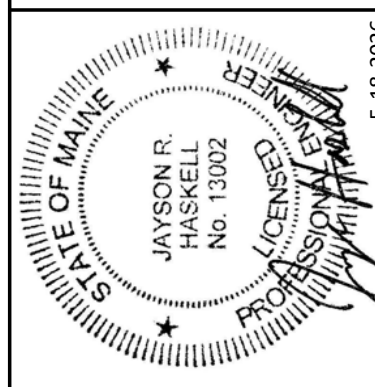


ROAD PROFILE: TURNING LEAF DRIVE

SCALE: HORIZ.: 1"=30'  
VERT.: 1"=3'GRAPHIC SCALE  
0 15 30 60

## LEGEND

EXISTING	PROPOSED
PROPERTY LINE/R.O.W.	PROPERTY LINE/R.O.W.
ABUTTER PROPERTY LINE	ABUTTER PROPERTY LINE
EASEMENT LINE	EASEMENT LINE
CENTERLINE	CENTERLINE
BUILDING	BUILDING
EDGE OF PAVEMENT/CURB	EDGE OF PAVEMENT/CURB
EDGE OF GRAVEL	EDGE OF GRAVEL
EDGE OF CONCRETE	EDGE OF CONCRETE
SIGN	SIGN
CONTOUR LINE	CONTOUR LINE
TREELINE	TREELINE
RETAINING WALL	RETAINING WALL
TEST PIT	TEST PIT
CATCHBASIN	CATCHBASIN
DRAINAGE MANHOLE	DRAINAGE MANHOLE
CULVERT/STORMDRAIN	CULVERT/STORMDRAIN
UNDERDRAIN	UNDERDRAIN
SEWER MANHOLE	SEWER MANHOLE
SANITARY SEWER PIPE	SANITARY SEWER PIPE
FORCE MAIN	FORCE MAIN
WATER MAIN	WATER MAIN
WATER VALVE	WATER VALVE
HYDRANT	HYDRANT
AIR RELEASE	AIR RELEASE
UTILITY POLE	UTILITY POLE
LIGHT POLE	LIGHT POLE
OVERHEAD UTILITIES	OVERHEAD UTILITIES
UNDERGROUND UTILITIES	UNDERGROUND UTILITIES
TRANSFORMER PAD	TRANSFORMER PAD
GAS MAIN	GAS MAIN
GAS VALVE	GAS VALVE
RIPRAP	RIPRAP
SILT FENCE	SILT FENCE

PERMITTING PLAN  
NOT FOR  
CONSTRUCTIONDM ROMA  
CONSULTING ENGINEERSP.O. BOX 1116  
WINDHAM, ME 04062  
(207) 591-5055

REV	DATE	BY	DESCRIPTION
A	9-25-24	JRH	ISSUED FOR MDP PERMIT REVIEW
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C	1-22-25	JRH	REVISED PER PWD REVIEW
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E	11-17-25	JRH	REVISED PER TOWN REVIEW COMMENTS
F	2-5-26	DMR	REVISED PER TOWN REVIEW COMMENTS
G	5-18-26	DMR	FINAL PLAN REVIEW

## ROADWAY PLAN &amp; PROFILE: TURNING LEAF DRIVE

## TURNING LEAF HEIGHTS

TURNING LEAF DRIVE, DRIVE IN LANE & COMMONS AVENUE  
WINDHAM, MAINEFOR RECORD OWNER:  
GATEWAY DEVELOPMENT, LLC  
4 COMMONS AVENUE, SUITE 12  
WINDHAM, ME 0406220013  
JOB NUMBER:1" = 30'  
SCALE:5-18-2026  
DATE:

SHEET 13 OF 19

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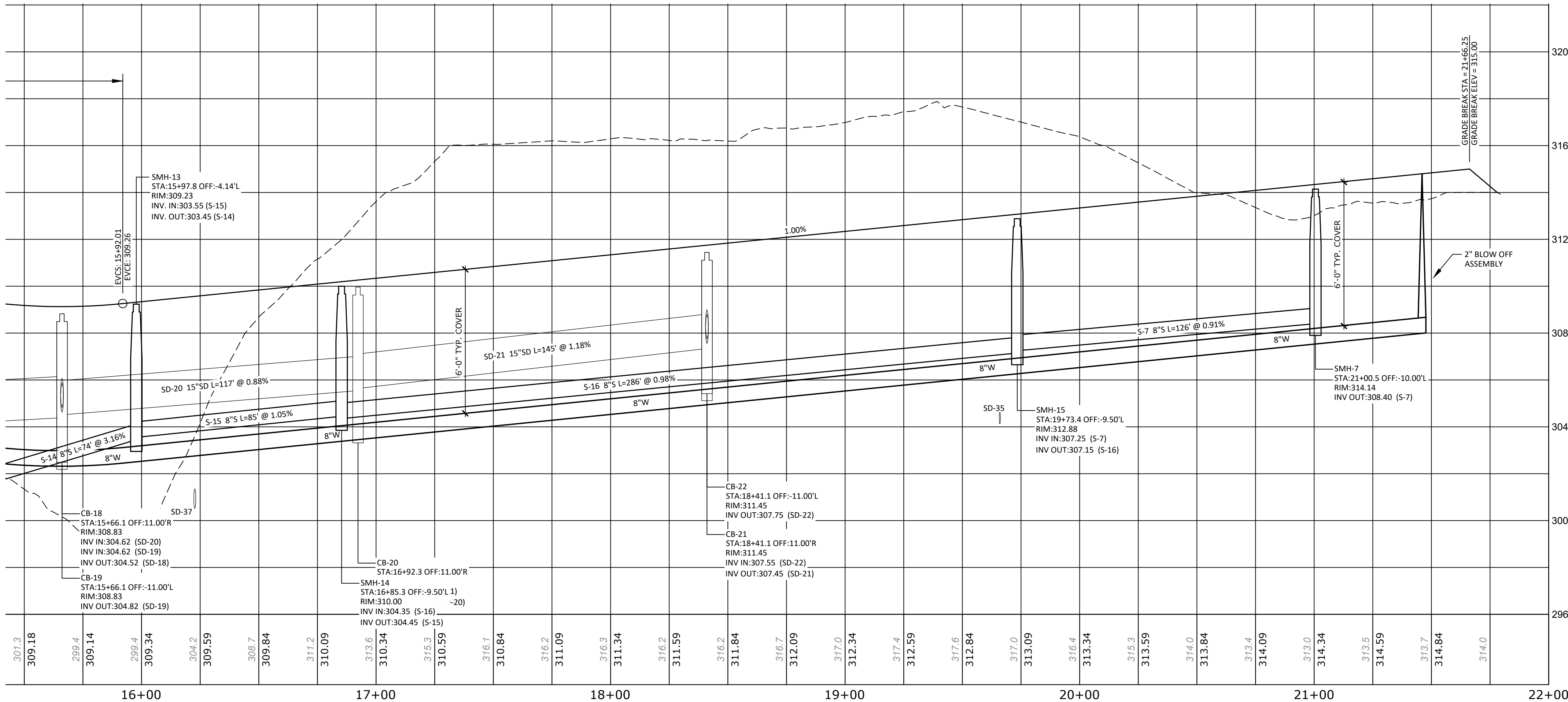
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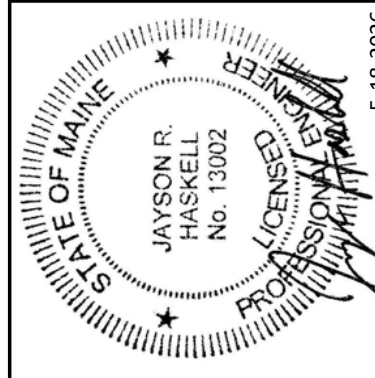


LEGEND

EXISTING	PROPOSED
PROPERTY LINE/R.O.W.	PROPERTY LINE/R.O.W.
ABUTTER PROPERTY LINE	ABUTTER PROPERTY LINE
EASEMENT LINE	EASEMENT LINE
CENTERLINE	CENTERLINE
BUILDING	BUILDING
EDGE OF PAVEMENT/CURB	EDGE OF PAVEMENT/CURB
EDGE OF GRAVEL	EDGE OF GRAVEL
EDGE OF CONCRETE	EDGE OF CONCRETE
SIGN	SIGN
CONTOUR LINE	CONTOUR LINE
TREE LINE	TREE LINE
RETAINING WALL	RETAINING WALL
TEST PIT	TEST PIT
CATCH BASIN	CATCH BASIN
DRAINAGE MANHOLE	DRAINAGE MANHOLE
CULVERT/STORM DRAIN	CULVERT/STORM DRAIN
UNDER DRAIN	UNDER DRAIN
SEWER MANHOLE	SEWER MANHOLE
SANITARY SEWER PIPE	SANITARY SEWER PIPE
FORCE MAIN	FORCE MAIN
WATER MAIN	WATER MAIN
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GAS MAIN	GAS MAIN
GAS VALVE	GAS VALVE
RIPRAP	RIPRAP
SILT FENCE	SILT FENCE



PERMITTING PLAN  
NOT FOR  
CONSTRUCTION



**DM ROMA**  
CONSULTING ENGINEERS

P.O. BOX 1116  
WINDHAM, ME 04062  
(207) 591-5055

REV	DATE	BY	DESCRIPTION
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D	11-17-25	JRH	REVISED PER TOWN REVIEW COMMENTS
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F	5-18-26	DMR	FINAL PLAN REVIEW

ROADWAY PLAN & PROFILE: TURNING LEAF DRIVE

TURNING LEAF HEIGHTS

TURNING LEAF DRIVE AND DRIVE-IN LANE  
WINDHAM, MAINE

FOR:  
GATEWAY DEVELOPMENT, LLC  
4 COMBANDS AVENUE - SUITE 12  
WINDHAM, ME 04062

20013  
JOB NUMBER:

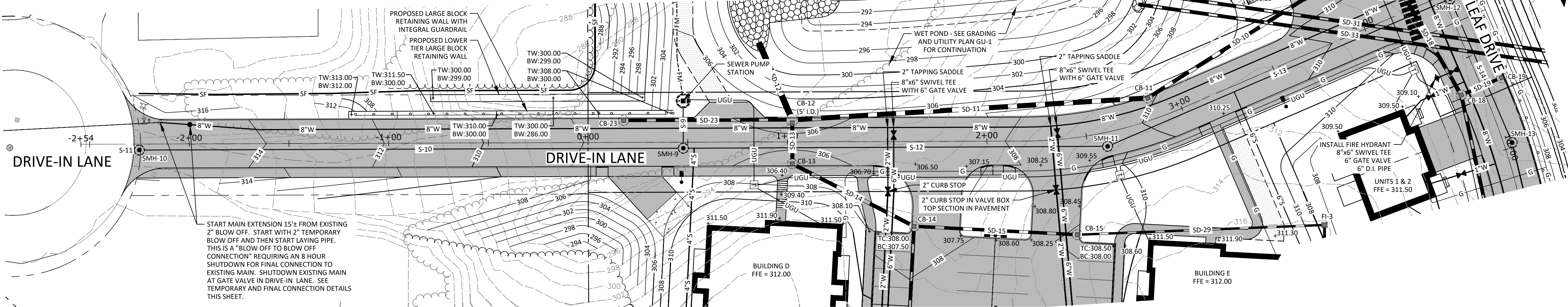
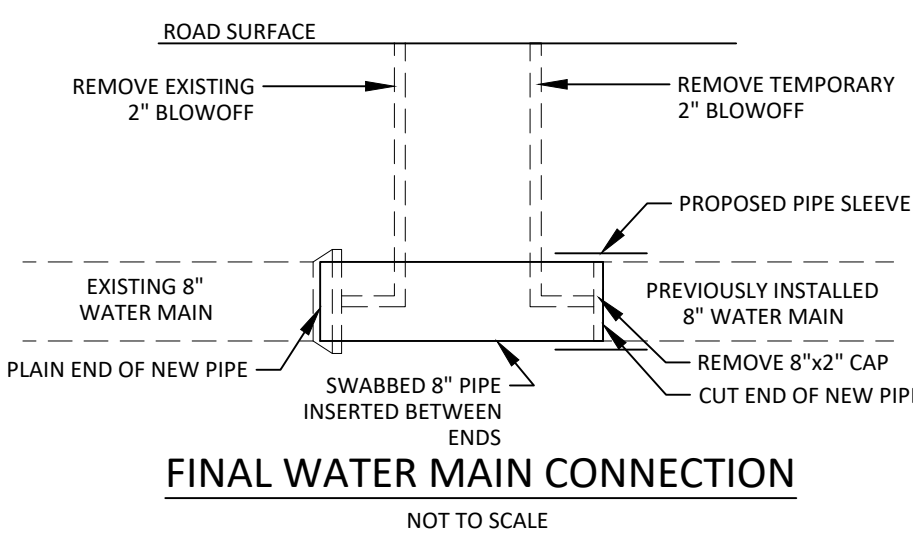
1" = 30'  
SCALE:

5-18-2026  
DATE:

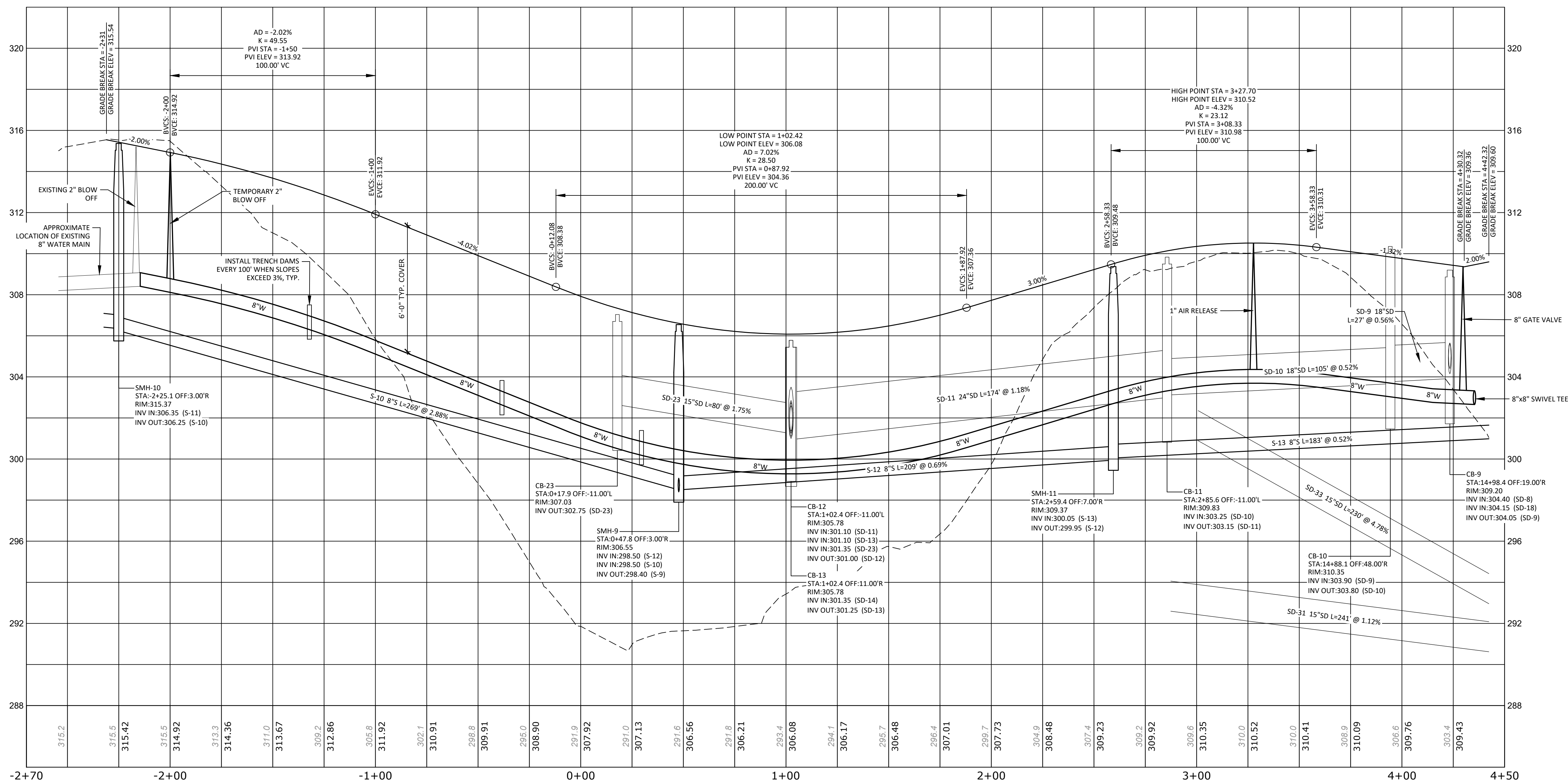
SHEET 14 OF 19

PP-3

1. HORIZONTAL DATUM: MAINE STATE PLANE, WEST ZONE, NAD83, U.S. FEET BASED ON BOUNDARY AND TOPOGRAPHIC SURVEY OF TURNING LAKE, WINDHAM, MAINE FOR BAKER BREAD CREAMERY, LLC AS PREPARED BY SURVEY, INC.
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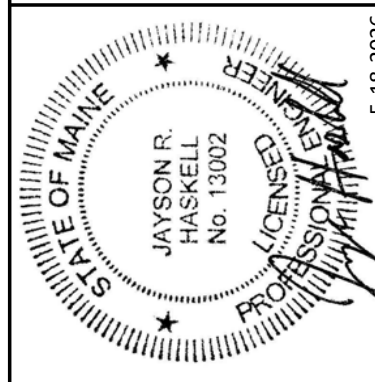


ROAD PLAN VIEW: DRIVE-IN LANE



EXISTING		PROPOSED
---	PROPERTY LINE/R.O.W.	---
---	ABUTTER PROPERTY LINE	---
---	EASEMENT LINE	---
---	CENTERLINE	---
	BUILDING	
---	EDGE OF PAVEMENT/CURB	
---	EDGE OF GRAVEL	---
---	EDGE OF CONCRETE	---
---	SIGN	
---200'---201'---	CONTOUR LINE	---200'---201'---
	TREELINE	
---	RETAINING WALL	---
TP-1	TEST PIT	---
	CATCHBASIN	
①	DRAINAGE MANHOLE	
---	CULVERT/STORMDRAIN	
---	UNDERDRAIN	---
②	SEWER MANHOLE	
S	SANITARY SEWER PIPE	S
---FM---	FORCE MAIN	---FM---
---	WATER MAIN	---
	WATER VALVE	
	HYDRANT	
---	AIR RELEASE	●
	UTILITY POLE	●
☆	LIGHT POLE	■ ● ☆
OHU	OVERHEAD UTILITIES	---
UGU	UNDERGROUND UTILITIES	UGU
---	TRANSFORMER PAD	
G	GAS MAIN	G
	GAS VALVE	
	RIPRAP	
---	SILT FENCE	---SF---

PERMITTING PLAN  
NOT FOR  
CONSTRUCTION



**DM ROMA**  
CONSULTING ENGINEERS  
P.O. BOX 1116  
WINDHAM, ME 04062

REV	DATE	BY	DESCRIPTION
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E		DMR	FINAL PLAN REVIEW
	5-18-26		

## ROADWAY PLAN &amp; PROFILE - DRIVE-IN LANE

TURNING LEAF HEIGHTS  
TURNING LEAF DRIVE AND DRIVE-IN LANE  
WINDHAM, MAINE

FOR:  
GATEWAY DEVELOPMENT, LLC  
4 COMMONS AVENUE - SUITE 12

20013  
JOB NUMBER:  
1" = 30'  
SCALE:  
5-18-2026  
DATE:

EET 15 OF 19

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## EROSION AND SEDIMENTATION CONTROL NOTES:

EXCAVATION AND EARTHWORK SHALL BE COMPLETED SUCH THAT NO MORE THAN 1 ACRE OF THE SITE IS WITHOUT STABILIZATION AT ANY ONE TIME. LIMIT THE EXPOSED AREA TO THOSE AREAS IN WHICH WORK IS TO OCCUR DURING THE FOLLOWING 15 DAYS AND THAT CAN BE MULCHED IN ONE DAY.

IN ORDER TO EFFECTIVELY PREVENT AND CONTROL EROSION RELATED TO SOIL DISTURBANCE, THE FOLLOWING BEST MANAGEMENT PRACTICES (BMPs) SHALL BE EMPLOYED:

### 1. POLLUTION PREVENTION

MINIMIZE DISTURBED AREAS AND PROTECT NATURAL DOWNGRADE BUFFER AREAS TO THE EXTENT PRACTICABLE. CONTROL STORMWATER VOLUME AND VELOCITY WITHIN THE SITE TO MINIMIZE SOIL EROSION. MINIMIZE THE DISTURBANCE OF STEEP SLOPES. CONTROL STORMWATER DISCHARGES, INCLUDING BOTH PEAK FLOW RATES AND VOLUME, TO MINIMIZE EROSION AT OUTLETS. THE DISCHARGE MAY NOT RESULT IN EROSION OF ANY OPEN DRAINAGE CHANNELS, SWALES, STREAM CHANNELS OR STREAM BANKS, UPLAND, OR COASTAL OR FRESHWATER WETLANDS OFF THE PROJECT SITE.

WHENEVER PRACTICABLE, NO DISTURBANCE ACTIVITIES SHOULD TAKE PLACE WITHIN 50 FEET OF ANY PROTECTED NATURAL RESOURCE. IF DISTURBANCE ACTIVITIES TAKE PLACE BETWEEN 30 FEET AND 50 FEET OF ANY PROTECTED NATURAL RESOURCE, AND STORMWATER DISCHARGES THROUGH THE DISTURBED AREAS TOWARD THE PROTECTED NATURAL RESOURCE, PERIMETER EROSION CONTROLS MUST BE DOUBLED. IF DISTURBANCE ACTIVITIES TAKE PLACE LESS THAN 30 FEET FROM ANY PROTECTED NATURAL RESOURCE, AND STORMWATER DISCHARGES THROUGH THE DISTURBED AREAS TOWARD THE PROTECTED NATURAL RESOURCE, PERIMETER EROSION CONTROLS MUST BE DOUBLED AND DISTURBED AREAS MUST BE TEMPORARILY OR PERMANENTLY STABILIZED WITHIN 7 DAYS.

### 2. TEMPORARY SOIL STABILIZATION BMPs

TEMPORARY MULCHING SHALL BE APPLIED IMMEDIATELY TO ANY AREAS THAT HAVE BEEN TEMPORARILY OR PERMANENTLY SEEDED. ANY DISTURBED SOIL WITHIN 75' OF A STREAM, WATER BODY OR WETLAND MUST RECEIVE TEMPORARY MULCH WITHIN 48 HOURS FOLLOWING DISTURBANCE AND BEFORE ANY STORM EVENT. OTHER AREAS SHALL RECEIVE TEMPORARY MULCHES WITHIN 7 DAYS OF DISTURBANCE. AREAS WHICH CANNOT BE SEEDED DURING THE GROWING SEASON SHALL BE MULCHED FOR OVER-WINTER PROTECTION. THE FOLLOWING ARE ACCEPTABLE TEMPORARY MULCHING METHODS:

HAY OR STRAW MULCHES NEED TO BE AIR-DRIED, FREE OF UNDESIRABLE SEEDS AND COARSE MATERIALS. APPLICATION RATE MUST BE 2 BALES (70-90 POUNDS) PER 1000 SQ FT OR 1.5 TO 2 TONS (90-100 BALES) PER ACRE TO COVER 75-90% OF THE GROUND SURFACE. HAY OR STRAW CAN BE DRIVEN INTO THE GROUND WITH TRACKED EQUIPMENT IF SLOPES ARE LESS THAN 3%, OR CAN BE ANCHORED WITH JUTE, WOOD FIBER OR PLASTIC NETTING ON STEEPER SLOPES.

EROSION CONTROL MIX MUST CONSIST PRIMARILY OF ORGANIC MATERIAL AND WILL INCLUDE ANY OF THE FOLLOWING: SHREDDED BARK, STUMP GRINDINGS, COMPOSTED BARK OR OTHER ACCEPTABLE PRODUCTS BASED ON A SIMILAR RAW SOURCE. WOOD OR BARK CHIPS, GROUND CONSTRUCTED WOOD PRODUCTS ARE NOT ACCEPTABLE. EROSION CONTROL MIX CAN BE USED AS A STAND-ALONE REINFORCEMENT ON SLOPES OF 2 HORIZONTAL TO 1 VERTICAL OR LESS AND DRAINING IN SHEET FLOW. IT CAN BE PLACED WITH A HYDRAULIC BUCKET, WITH A PNEUMATIC BLOWER OR BY HAND, AND MUST PROVIDE 100% SOIL COVERAGE.

EROSION CONTROL MIX SHALL MEET THE FOLLOWING SPECIFICATIONS:  
-ORGANIC MATTER CONTENT SHALL BE BETWEEN 80-100%, DRY WEIGHT BASIS.  
-PARTICLE SIZE BY WEIGHT SHALL BE 100% PASSING A 5 IN. SCREEN AND BETWEEN 70-85% PASSING 0.75 IN. SCREEN  
-ORGANIC PORTION NEEDS TO BE FIBROUS AND ELONGATED  
-LARGE PORTIONS OF SILTS, CLAYS OR FINE SANDS ARE NOT ACCEPTABLE IN THE MIX

WHEN USED AS MULCH, THE THICKNESS OF THE EROSION CONTROL MIX IS BASED UPON THE FOLLOWING:

LENGTH OF SLOPE	3:1 SLOPE OR LESS	BETWEEN 2:1 AND 3:1 SLOPE
LESS THAN 20 FT	2.0 IN.	4.0 IN.
BETWEEN 20 - 60 FT	3.0 IN.	5.0 IN.
BETWEEN 60 - 100 FT	4.0 IN.	6.0 IN.

CHEMICAL MULCHES AND SOIL BINDERS MAY BE USED AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL CONSULT WITH THE MANUFACTURER TO DETERMINE ADEQUATE APPLICATION RATES AND METHODS.

TEMPORARY MULCH SHALL BE INSPECTED FOLLOWING ANY SIGNIFICANT RAINFALL EVENT. IF LESS THAN 90% OF THE SOIL SURFACE IS COVERED BY MULCH, ADDITIONAL MULCH SHALL BE IMMEDIATELY APPLIED. EROSION CONTROL MATS AND MULCH ANCHORING MUST BE INSPECTED AFTER RAINFALL EVENTS FOR DISLOCATION OR FAILURE, AND REPAIRED IMMEDIATELY. INSPECTIONS SHALL TAKE PLACE UNTIL 95% OF THE SOIL SURFACE IS COVERED WITH PERMANENT VEGETATION. WHERE MULCH IS USED WITH ORNAMENTAL PLANTINGS, INSPECT PERIODICALLY THROUGHOUT THE YEAR TO DETERMINE IF MULCH IS MAINTAINING COVERAGE OF THE SOIL SURFACE, AND REPAIR AS NEEDED.

TEMPORARY VEGETATION SHALL BE ESTABLISHED ON SOILS THAT WILL NOT BE BROUGHT TO FINAL GRADE FOR A PERIOD OF MORE THAN 30 DAYS. IF TEMPORARY VEGETATION CANNOT BE ESTABLISHED PRIOR TO OCTOBER 15, TEMPORARY MULCH SHALL BE APPLIED THROUGH THE WINTER AND TEMPORARY VEGETATION SHALL BE PLANTED AT THE BEGINNING OF THE GROWING SEASON THE FOLLOWING YEAR. TO PREPARE THE SEEDED, THE CONTRACTOR SHALL APPLY FERTILIZER AT A RATE OF 600 POUNDS PER ACRE OF 10-10-10 (N-P205-K20) OR EQUIVALENT AND LIMESTONE AT A RATE OF 3 TONS PER ACRE, IF NECESSARY. LOOSEN SOIL TO A DEPTH OF 2 INCHES IN AREAS THAT HAVE BEEN COMPACTED BY CONSTRUCTION ACTIVITIES. GRASS SEED SHALL BE SELECTED BASED UPON THE TIME OF YEAR THE PLANTING WILL TAKE PLACE AS SUMMARIZED IN THE FOLLOWING TABLE:

SEED	LB. PER ACRE	RECOMMENDED SEEDING DATES
WINTER RYE	112	8/15 - 10/1
OATS	80	4/1 - 7/1 8/15 - 9/15
ANNUAL RYEGRASS	40	4/1 - 7/1

TEMPORARY SEEDING SHALL BE PERIODICALLY INSPECTED TO MAINTAIN AT LEAST 95% VEGETATIVE COVER OF SOIL SURFACE. IF ANY EVIDENCE OF EROSION OR SEDIMENTATION IS APPARENT, REPAIRS SHALL BE MADE AND OTHER TEMPORARY MEASURES SHALL BE USED IN THE INTERIM SUCH AS TEMPORARY MULCH, FILTER BARRIERS, ETC.

### 3. SEDIMENT BARRIER BMPs

PRIOR TO CONSTRUCTION TEMPORARY SEDIMENT BARRIERS SHALL BE INSTALLED AT THE DOWNGRADE EDGE OF ANY AREA TO BE DISTURBED AND ADJACENT TO ANY DRAINAGE CHANNELS WITHIN THE DISTURBED AREA. SEDIMENT BARRIERS INCLUDE ANY OF THE FOLLOWING:

FILTER BARRIER FENCE, ALSO CALLED SILT FENCE, SHALL BE INSTALLED WHERE SHOWN ON THE PLANS AND IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS. THE FILTER FABRIC SHALL BE A PEROUS SHEET OF PROPYLENE, NYLON, POLYESTER OR ETHYLENE YARN AND SHALL PROVIDE A MINIMUM OF 6 MONTHS USABLE CONSTRUCTION LIFE INCLUDING PROTECTION AGAINST ULTRA-VIOLET LIGHT. THE HEIGHT OF THE FENCE SHALL NOT EXCEED 36 INCHES INSTALLED AND POST SPACING SHALL NOT EXCEED 6 FEET. JOINTS IN THE FENCE SHALL BE AVOIDED TO THE EXTENT POSSIBLE, AND IF NECESSARY SHALL BE SPICED TOGETHER AT A SUPPORT POST WITH A MINIMUM 6 INCH OVERLAP. A TRENCH SHALL BE EXCAVATED APPROXIMATELY 6 INCHES WIDE AND 6 INCHES DEEP, AND THE BOTTOM 6-8 INCHES OF FABRIC SHALL BE "TOED-IN" TO THE TRENCH AND COMPACTED. THE TRENCH SHOULD BE UPHILL OF THE FABRIC PRIOR TO BURAL.

EROSION CONTROL MIX BERMS ARE LINEAR BARRIERS COMPOSED OF EROSION CONTROL MIX AS SPECIFIED ABOVE. THE BERM MUST BE A MINIMUM OF 12 INCHES TALL AND 24 INCHES WIDE AT THE BASE IF UPHILL SLOPES ARE LESS THAN 5%. STEEPER SLOPES OR SLOPES GREATER THAN 20 FEET LONG MAY REQUIRE A LARGER WIDTH BERM. EROSION CONTROL MIX BERMS SHALL BE PROHIBITED AT THE BASE OF A LONG OR STEEP SLOPE (8% OR GREATER) WITHOUT THE ADDITIONAL SUPPORT OF A FILTER FENCE INSTALLED ON THE DOWNHILL SIDE OF THE BERM.

SEDIMENT BARRIERS SHOULD BE INSTALLED DOWNGRADE OF SOIL OR SEDIMENT STOCKPILES AND STORMWATER PREVENTED RUNNING ONTO THE STOCKPILE. SEDIMENT BARRIERS SHALL BE INSPECTED AFTER ANY SIGNIFICANT RAINFALL EVENT AND REPAIRED IMMEDIATELY IF THERE ARE ANY SIGNS OF EROSION OR SEDIMENTATION BELOW THE BARRIERS. IF THERE ARE SIGNS OF UNDERCUTTING AT THE CENTER OR EDGES OF THE BARRIER, OR IF LARGE VOLUMES OF WATER ARE IMPOUNDED BEHIND THE BARRIER, IT MAY BE NECESSARY TO INSTALL A SEDIMENT BASIN UPSTREAM OF THE SEDIMENT BARRIER. SEDIMENT SHALL BE REMOVED ONCE IT REACHES HALF THE BARRIER HEIGHT. AFTER THE BARRIER IS REMOVED, ANY REMAINING SILT SHALL EITHER BE REMOVED OR GRADED TO CONFORM WITH THE EXISTING TOPOGRAPHY AND VEGETATED.

TEMPORARY EROSION CONTROL MEASURES ARE REMOVED ONCE THE SITE IS PERMANENTLY STABILIZED AND CONSTRUCTION ACTIVITY HAS BEEN COMPLETED.

### 4. STORM DRAIN INLET PROTECTION

STORM DRAIN INLETS THAT ARE MADE OPERATIONAL BEFORE THEIR DRAINAGE AREA IS STABILIZED SHALL BE PROTECTED WITH A FILTER UNTIL THE DRAINAGE AREA IS EITHER PAVED OR STABILIZED WITH 95% VEGETATIVE GROWTH. THE FOLLOWING ARE ACCEPTABLE BMPs ASSOCIATED WITH STORM DRAIN INLET PROTECTION:

MANUFACTURED SEDIMENT FILTERS ARE THE PREFERRED METHOD FOR PROTECTING CATCH BASIN INLETS IN PAVED OR GRAVEL ROADWAYS. THE FILTERS TYPICALLY CONSIST OF A FABRIC OR OTHER PEROUS MATERIAL THAT IS PLACED ABOVE OR BELOW THE GRATE THAT TRAPS SEDIMENT ON THE SURFACE AND ALLOWS WATER TO FLOW THROUGH THE GRATE. CONSIDERATIONS SUCH AS WEATHER CONDITIONS, SLOPES, TRIBUTARY WATERSHED AREA AND EXPECTED SEDIMENT ACCUMULATION SHOULD BE FACTORED INTO MAKING A DECISION ON ANY PARTICULAR PRODUCT, AND THE MANUFACTURER'S RECOMMENDATIONS ON INSTALLATION AND MAINTENANCE SHALL BE STRICTLY ADHERED TO.

### 5. STABILIZED CONSTRUCTION ENTRANCE/EXIT

TO REDUCE THE TRACKING OF SEDIMENT ONTO ROADWAYS, A STABILIZED CONSTRUCTION EXIT SHALL BE INSTALLED AT ALL POINTS OF EGRESS WHERE VEHICLES MAY TRAVEL FROM THE PROJECT SITE TO A PUBLIC ROAD OR OTHER PAVED AREA. THE STONE PAD SHALL CONSIST OF A MINIMUM 6-INCH DEPTH OF 2-3 INCH CRUSHED STONE, AND SHALL BE PLACED ON A GEOTEXTILE FABRIC. THE PAD SHALL EXTEND AT LEAST 50 FEET INTO THE PROJECT SITE AND BE A MINIMUM OF 10 FEET WIDE. THE EXIT SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY, AND THE CONTRACTOR SHALL SWEEP PAVEMENT AT EXITS THAT HAVE EXPERIENCED ANY MUD-TRACKING PRIOR TO THE NEXT STORM EVENT. MAINTAIN THE PAD UNTIL ALL DISTURBED AREAS ARE STABILIZED.

## INSPECTION & MAINTENANCE NOTES:

- THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE ALL CONSTRUCTION OPERATIONS COMPLY WITH THE INSPECTION AND MAINTENANCE PROCEDURES FOR THE PROJECT, INCLUDING, BUT NOT LIMITED TO THOSE INCLUDED IN THIS PLAN SET, THE "INSPECTION, MAINTENANCE, AND HOUSEKEEPING PLAN", AND THE "MAINE EROSION AND SEDIMENTATION CONTROL PRACTICES FIELD GUIDE FOR CONTRACTORS". INSPECTION SHALL OCCUR ON ALL DISTURBED AND IMPERVIOUS AREAS, EROSION CONTROL MEASURES, MATERIAL STORAGE AREAS THAT ARE EXPOSED TO PRECIPITATION, AND LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE. THESE AREAS SHALL BE INSPECTED AT LEAST ONCE A WEEK AS WELL AS 24 HOURS BEFORE AND AFTER A STORM EVENT GENERATING MORE THAN 0.5 INCH OF RAINFALL OVER A 24-HOUR PERIOD AND PRIOR TO COMPLETING PERMANENT STABILIZATION MEASURES. A PERSON WITH KNOWLEDGE OF EROSION AND STORMWATER CONTROL, INCLUDING THE STANDARDS AND CONDITIONS IN THE PERMIT, SHALL CONDUCT THE INSPECTIONS.
- EROSION CONTROLS SHALL BE MAINTAINED IN EFFECTIVE OPERATING CONDITION UNTIL AREAS ARE PERMANENTLY STABILIZED. IF BEST MANAGEMENT PRACTICES (BMPs) NEED TO BE REPAIRED, THE REPAIR WORK SHOULD BE INITIATED UPON DISCOVERY OF THE PROBLEM BUT NO LATER THAN THE END OF THE NEXT WORKDAY. IF BMPs NEED TO BE MAINTAINED OR MODIFIED, ADDITIONAL BMPs ARE NECESSARY, OR OTHER CORRECTIVE ACTION IS NEEDED, IMPLEMENTATION MUST BE COMPLETED WITHIN SEVEN CALENDAR DAYS AND PRIOR TO ANY RAINFALL EVENT.
- A REPORT SUMMARIZING THE INSPECTIONS AND ANY CORRECTIVE ACTION TAKEN MUST BE MAINTAINED ON SITE. THE LOG MUST INCLUDE THE NAME(S) AND QUALIFICATIONS OF THE PERSON MAKING THE INSPECTIONS; THE DATE(S) OF THE INSPECTIONS; AND THE MAJOR OBSERVATIONS ABOUT THE OPERATION AND MAINTENANCE OF EROSION AND SEDIMENTATION CONTROLS, MATERIALS STORAGE AREAS, AND VEHICLE ACCESS POINTS TO THE PARCEL. MAJOR OBSERVATIONS MUST INCLUDE BMPs THAT NEED MAINTENANCE, BMPs THAT FAILED TO OPERATE AS DESIGNED OR PROVED INADEQUATE FOR A PARTICULAR LOCATION, AND LOCATION(S) WHERE ADDITIONAL BMPs ARE NEEDED. FOR EACH BMP REQUIRING MAINTENANCE, BMP NEEDING REPLACEMENT, AND LOCATION NEEDING ADDITIONAL BMPs, NOTE IN THE LOG THE CORRECTIVE ACTION TAKEN AND WHEN IT WAS TAKEN. THE LOG MUST BE MADE ACCESSIBLE TO INDEX AND TOWN STAFF, AND A COPY MUST BE PROVIDED UPON REQUEST. THE OWNER SHALL RETAIN A COPY OF THE LOG FOR A PERIOD OF AT LEAST THREE YEARS FROM THE COMPLETION OF PERMANENT STABILIZATION.

### 6. DUST CONTROL

THE CONTRACTOR IS RESPONSIBLE FOR CONTROLLING DUST ON THE PROJECT SITE AND ON ADJACENT ROADWAYS. EXPOSED SOIL SURFACES SHALL BE MOISTENED PERIODICALLY WITH ADEQUATE WATER TO CONTROL DUST. GRAVEL SURFACES SHALL EITHER BE TREATED WITH AN APPLICATION OF CALCIUM CHLORIDE OR COVERED WITH CRUSHED STONE IF DUST CONTROL BECOMES DIFFICULT WITH NORMAL WATER APPLICATIONS.

### 7. LAND GRADING AND SLOPE PREPARATION

GRADING SHALL BE PLANNED SO AS TO MINIMIZE THE LENGTH OF TIME BETWEEN INITIAL SOIL EXPOSURE AND FINAL GRADING. ON LARGE PROJECTS THIS SHOULD BE ACCOMPLISHED BY PHASING THE OPERATION AND COMPLETING THE FIRST PHASE UP TO FINAL GRADING AND SEEDING BEFORE STARTING THE NEXT PHASE. ANY EXPOSED AREA THAT WILL NOT BE FINISH GRADED WITHIN 7 DAYS SHALL BE TREATED WITH MULCH OR PLANTED WITH TEMPORARY VEGETATION. PROVISIONS SHALL BE MADE TO SAFELY CONVEY SURFACE RUNOFF TO STORM DRAINS, PROTECTED OUTLETS OR TO STABLE WATER COURSES TO ENSURE THAT SURFACE RUNOFF WILL NOT DAMAGE SLOPES OR OTHER GRADED AREAS. CUT AND FILL SLOPES THAT ARE TO BE STABILIZED WITH GRASS SHALL NOT BE STEEPER THAN 2:1. AREAS TO BE FILLED SHALL BE CLEARED, GRUBBED AND STRIPPED OF TOPSOIL TO REMOVE TREES, VEGETATION, ROOTS OR OTHER OBJECTIONABLE MATERIALS. AREAS SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 3 INCHES PRIOR TO PLACEMENT OF TOPSOIL. ALL FILLS SHALL BE COMPACTED AS REQUIRED TO REDUCE EROSION, SLURRING, SETTLEMENT OR OTHER RELATED PROBLEMS. FILL INTENDED TO SUPPORT BUILDINGS, STRUCTURES AND CONDUITS, ETC. SHALL BE COMPACTED IN ACCORDANCE WITH LOCAL REQUIREMENTS OR CODES. ALL FILLS SHALL BE PLACED AND COMPACTED IN LAYERS NOT TO EXCEED 8 INCHES IN THICKNESS. FILL MATERIAL SHALL BE FREE OF STUMPS, BUILDING DEBRIS AND OTHER OBJECTIONABLE MATERIALS THAT WOULD INTERFERE WITH OR PREVENT CONSTRUCTION OF SATISFACTORY LIFTS. FROZEN MATERIAL OR SOFT, MUCKY OR HIGHLY COMPRESSIBLE MATERIALS SHALL NOT BE INCORPORATED INTO FILL SLOPES OR STRUCTURAL FILLS. FILL SHALL NOT BE PLACED ON A FROZEN FOUNDATION. SEEPS OR SPRINGS ENCOUNTERED DURING CONSTRUCTION SHALL BE HANDLED APPROPRIATELY. ALL GRADED AREAS SHALL BE PERMANENTLY STABILIZED IMMEDIATELY FOLLOWING FINISHED GRADING.

### 8. TOPSOIL

IF POSSIBLE, TOPSOIL SHALL BE STOCKPILED ON THE PROJECT SITE AND REUSED. HIGH QUALITY TOPSOIL SHALL BE FRIABLE AND LOAMY (LOAM, SANDY LOAM, SILT LOAM, SANDY CLAY LOAM, CLAY LOAM), AND SHALL BE FREE OF DEBRIS, TRASH, STUMPS, ROCKS, ROOTS AND NOXIOUS WEEDS. AFTER THE AREAS TO BE TOPSOILED HAVE BEEN BROUGHT TO GRADE, AND IMMEDIATELY PRIOR TO SPREADING THE TOPSOIL, THE SUBGRADE SHALL BE LOOSENED BY SCARIFYING TO A DEPTH OF AT LEAST 2 INCHES TO ENSURE BONDING WITH SUBSOIL. THE TOPSOIL SHALL BE UNIFORMLY DISTRIBUTED TO A MINIMUM COMPACTED DEPTH OF 4 INCHES. ANY IRREGULARITIES IN THE SURFACE RESULTING FROM TOPSOILING OR OTHER OPERATIONS SHALL BE CORRECTED IN ORDER TO PREVENT THE FORMATION OF DEPRESSIONS OR WATER POCKETS. IT IS NECESSARY TO COMPACT THE TOPSOIL ENOUGH TO ENSURE GOOD CONTACT WITH THE UNDERLYING SOIL, BUT UNDUCE COMPACTION IS TO BE AVOIDED.

### 9. PERMANENT SOIL STABILIZATION

IF THE AREA WILL NOT BE WORKED FOR MORE THAN ONE YEAR OR HAS BEEN BROUGHT TO FINAL GRADE, THEN PERMANENTLY STABILIZE THE AREA WITHIN 7 DAYS BY PLANTING VEGETATION, SEEDING, SOD, OR THROUGH THE USE OF PERMANENT MULCH, OR RIPRAP, OR ROAD SUB-BASE. IF USING VEGETATION FOR STABILIZATION, SELECT THE PROPER VEGETATION FOR THE LIGHT, MOISTURE, AND SOIL CONDITIONS; AMEND AREAS OF DISTURBED SUBSOILS WITH TOPSOIL, COMPOST, OR FERTILIZERS; PROTECT SEEDED AREAS WITH MULCH OR, IF NECESSARY, EROSION CONTROL BLANKETS; AND SCHEDULE SODDING, PLANTING, AND SEEDING SO TO AVOID DIE-OFF FROM SUMMER DROUGHT AND FALL FROSTS. NEWLY SEEDED OR SODDED AREAS MUST BE PROTECTED FROM VEHICLE TRAFFIC, EXCESSIVE PEDESTRIAN TRAFFIC, AND CONCENTRATED RUNOFF UNTIL THE VEGETATION IS WELL-ESTABLISHED WITH 90% COVER BY HEALTHY VEGETATION. IF NECESSARY, AREAS MUST BE REWORKED AND REESTABLISHED IF GERMINATION IS SPARSE, PLANT COVERAGE IS SPOTTY, OR TOPSOIL EROSION IS EVIDENT. ONE OR MORE OF THE FOLLOWING MAY APPLY TO A PARTICULAR SITE.

SEEDED AREAS: TO PREPARE THE SEEDED, APPLY 10-20-20 FERTILIZER AT A RATE OF 800 POUNDS PER ACRE AND GROUND LIMESTONE AT A RATE OF 3 TONS PER ACRE. WORK THE FERTILIZER AND LIMESTONE INTO THE TOPSOIL TO A DEPTH OF 4 INCHES AND REMOVE ANY STONES, ROOTS OR OTHER VISIBLE DEBRIS. SELECT A SEED MIXTURE THAT IS APPROPRIATE FOR THE SOIL TYPE AND MOISTURE CONTENT AS FOUND AT THE SITE, AND FOR THE AMOUNT OF SUN EXPOSURE AND FOR LEVEL OF USE. REFER TO THE USDA SOIL AND WATER CONSERVATION SERVICE OR THE LOCAL SOIL AND WATER CONSERVATION DISTRICT FOR APPROPRIATE SEED MIXTURES. APPLY SEED UNIFORMLY IN ACCORDANCE WITH SUPPLIER RECOMMENDATIONS AND IMMEDIATELY COVER WITH MULCH AS DESCRIBED IN THE TEMPORARY MULCHING SECTION OF THIS PLAN.

HYDROSEEDING SHALL BE DONE IN ACCORDANCE WITH SUPPLIER'S RECOMMENDATIONS. FOR SEEDED AREAS TO BE PERMANENTLY STABILIZED, 90% OF THE DISTURBED SOIL SHALL BE COVERED WITH MATURE HEALTHY PLANTS WITH NO EVIDENCE OF WASHING OR RILLING OF THE TOPSOIL.

SOD STRIPS SHALL BE LAID AT RIGHT ANGLES TO DIRECTION OF SLOPE OR FLOW OF WATER STARTING AT LOWEST ELEVATION. JOINTS SHALL BE STAGGERED, AND ALL STRIPS SHALL BE ROLLED OR TAMPED INTO PLACE. ON SLOPES, SOD SHALL BE ANCHORED WITH STAPLES, WIRE OR PINS. IRRIGATE SODDED AREA IMMEDIATELY AFTER INSTALLATION. FOR SODDED AREAS TO BE PERMANENTLY STABILIZED, THE ROOTS OF THE SOD MUST BE COMPLETELY BOUND INTO THE UNDERLYING SOIL WITH NO SLUMPING OF THE SOD OR DIE-OFF.

PERMANENT MULCH IS A LONG TERM COVER THAT PROVIDES A GOOD BUFFER AROUND DISTURBED AREAS. THE EROSION CONTROL MIX SHALL CONSIST PRIMARILY OF ORGANIC MATERIAL AND MAY INCLUDE SHREDDED BARK, STUMP GRINDINGS OR COMPOSTED BARK. WOOD CHIPS, GROUND CONSTRUCTION DEBRIS, REPROCESSED WOOD PRODUCTS ARE NOT ACCEPTABLE. THE EROSION CONTROL MIX SHALL CONTAIN A WELL-GRADED MIXTURE OF PARTICLE SIZES AND MAY CONTAIN ROCKS LESS THAN 4 INCHES IN DIAMETER. EROSION CONTROL MIX MUST BE FREE OF REFUSE, PHYSICAL CONTAMINANTS AND MATERIAL TOXIC TO PLANT GROWTH.

RIPRAP STONE SHALL CONSIST OF SUB-ANGULAR FIELD STONE OR ROUGH UNEVEN QUARRY STONE OF APPROXIMATELY RECTANGULAR SHAPE. THE DEPTH OF STONE SHALL BE A MINIMUM OF 2.2 TIMES THE MAXIMUM STONE DIAMETER. A GRAVEL OR GEOTEXTILE FILTER BLANKET SHALL BE PLACED BETWEEN THE RIPRAP AND UNDERLYING SOIL SURFACE. GRAVEL FILTER BLANKETS SHALL MEET MOOT TYPE-C UNDERDRAIN MATERIAL SPECIFICATIONS AND BE AT LEAST 6 INCHES THICK. GEOTEXTILE FILTER BLANKETS SHALL BE SPECIFIED BASED ON SITE CONDITIONS. RIPRAP SLOPES SHALL BE TOED INTO THE BASE OF THE EMBANKMENT BY EXCAVATING A TRENCH AT THE BOTTOM OF THE SLOPE AND INSTALLING A STABLE BASE OF RIPRAP TO GRADE.

DITCHES, CHANNELS AND SWALES ARE CONSIDERED PERMANENTLY STABILIZED WHEN THE CHANNEL HAS 90% COVER OF HEALTHY VEGETATION WITH A WELL GRADED RIPRAP LINING, EROSION CONTROL BLANKET, OR WITH ANOTHER NON-EROSIVE LINING SUCH AS CONCRETE OR ASPHALT PAVEMENT. THERE MUST BE NO EVIDENCE OF SLUMPING OF THE CHANNEL LINING, UNDERCUTTING OF THE BANKS, OR DOWNCUTTING OF THE CHANNEL.

### 10. STORMWATER CHANNELS

EACH CHANNEL SHOULD BE CONSTRUCTED IN SECTIONS SO THAT THE SECTION'S GRADING, SHAPING, AND INSTALLATION OF THE PERMANENT LINING CAN BE COMPLETED THE SAME DAY. IF A CHANNEL'S FINAL GRADING OR LINING INSTALLATION MUST BE DELAYED, THEN DIVERSION BERMS MUST BE USED TO DIVERT STORMWATER AWAY FROM THE CHANNEL. PROPERLY-SPACED CHECK DAMS MUST BE INSTALLED IN THE CHANNEL TO SLOW THE WATER VELOCITY, AND A TEMPORARY LINING INSTALLED ALONG THE CHANNEL TO PREVENT SCOURING.

## WINTER EROSION AND SEDIMENTATION CONTROL NOTES:

THE WINTER CONSTRUCTION PERIOD TYPICALLY BEGINS IN EARLY NOVEMBER AND ENDS IN MID APRIL. IF A CONSTRUCTION SITE IS NOT STABILIZED WITH PAVEMENT, A ROAD GRAVEL BASE OR RIPRAP BY NOVEMBER 15 THEN THE SITE NEEDS TO BE PROTECTED WITH OVER-WINTER STABILIZATION. WINTER EXCAVATION AND EARTHWORK SHALL BE COMPLETED SUCH THAT NO MORE THAN 1 ACRE OF THE SITE IS WITHOUT STABILIZATION AT ANY ONE TIME. LIMIT THE EXPOSED AREA TO THOSE AREAS IN WHICH WORK IS TO OCCUR DURING THE FOLLOWING 15 DAYS AND THAT CAN BE MULCHED IN ONE DAY PRIOR TO ANY SNOW EVENT. AN AREA SHALL BE CONSIDERED DENuded UNTIL THE SUBBASE GRAVEL IS INSTALLED IN THE ROADWAY AREAS OR THE AREAS OF FUTURE LOAM AND SEED HAVE BEEN LOADED, SEEDED AND MULCHED. A COVER OR EROSION CONTROL MIX IS THE PREFERRED TEMPORARY MULCH DURING WINTER CONDITIONS.

#### 1. NATURAL RESOURCE PROTECTION

ANY AREAS WITHIN 75 FEET FROM ANY REGULATED NATURAL RESOURCES SHALL BE MULCHED BY DECEMBER 1 AND ANCHORED WITH PLASTIC NETTING OR PROTECTED WITH AN EROSION CONTROL COVER. DURING WINTER CONSTRUCTION, A DOUBLE ROW OF SEDIMENT BARRIERS (FOR EXAMPLE, SILT FENCE BACKED WITH HAY BALES OR EROSION CONTROL MIX) WILL BE PLACED BETWEEN ANY REGULATED NATURAL RESOURCE AND THE DISTURBED AREA. PROJECTS CROSSING THE REGULATED NATURAL RESOURCE SHALL BE PROTECTED A MINIMUM DISTANCE OF 100 FEET ON EITHER SIDE FROM THE RESOURCE. EXISTING PROJECTS NOT STABILIZED BY DECEMBER 1 SHALL BE PROTECTED WITH THE SECOND LINE OF SEDIMENT BARRIER TO ENSURE FUNCTIONALITY DURING THE SPRING THAW AND RAINS.

#### 2. SEDIMENT BARRIERS

DURING FROZEN CONDITIONS, SEDIMENT BARRIERS MAY CONSIST OF EROSION CONTROL MIX BERMS OR ANY OTHER RECOGNIZED SEDIMENT BARRIERS AS FROZEN SOIL PREVENTS THE PROPER INSTALLATION OF HAY BALES OR SILT FENCES.

#### 3. MULCHING

ALL AREAS SHALL BE CONSIDERED TO BE DENuded UNTIL SEEDED AND MULCHED. HAY AND STRAW MULCH SHALL BE APPLIED AT A RATE OF 3 TONS PER ACRE (TWICE THE NORMAL ACCEPTED RATE) AND SHALL BE PROPERLY ANCHORED. EROSION CONTROL MIX MUST BE APPLIED WITH A MINIMUM 4 INCHES THICKNESS. MULCH SHALL NOT BE SPREAD ON TOP OF SNOW. SNOW MUST BE REMOVED DOWN TO A ONE-INCH DEPTH PRIOR TO APPLICATION. AFTER EACH DAY OF FINAL GRADING, THE AREA WILL BE PROPERTY STABILIZED WITH ANCHORED HAY OR STRAW OR EROSION CONTROL MATTING. AN AREA SHALL BE CONSIDERED TO HAVE BEEN STABILIZED WHEN EXPOSED SURFACES ARE COVERED EITHER WITH MULCH OR WITH A COVERED SO THAT GROUND SURFACE IS NOT VISIBLE THROUGH THE MULCH. BETWEEN THE DATES OF NOVEMBER 1 AND APRIL 15, ALL MULCH SHALL BE ANCHORED BY EITHER MULCH NETTING, TRACKING OR WOOD CELLULOSE FIBER. THE COVER WILL BE CONSIDERED SUFFICIENT WITH THE GROUND SURFACE IS NOT VISIBLE THROUGH THE MULCH. AFTER NOVEMBER 15, MULCH AND ANCHORING OF ALL EXPOSED SOIL SHALL OCCUR AT THE END OF EACH FINAL GRADING WORKDAY.

#### 4. SOIL STOCKPILING

STOCKPILES OF SOIL OR SUBSOIL WILL BE MULCHED FOR OVER WINTER PROTECTION WITH HAY OR STRAW AT TWICE THE NORMAL RATE FOR WITH A FOUR-INCH LAYER OF EROSION CONTROL MIX. THIS WILL BE DONE WITHIN 24 HOURS OF STACKING AND RE-ESTABLISHED PRIOR TO ANY RAINFALL OR SNOWFALL. ANY SOIL STOCKPILE WILL NOT BE PLACED WITHIN 100 FEET FROM ANY REGULATED NATURAL RESOURCE.

#### 5. SEEDING

BETWEEN THE DATES OF OCTOBER 15 AND APRIL 1, LOAM OR SEED WILL NOT BE REQUIRED. DURING PERIODS OF ABOVE FREEZING TEMPERATURES FINISHED AREAS SHALL BE FINISH GRADED AND MULCHED OR TEMPORARILY SEEDED AND MULCHED UNTIL SUCH TIME AS THE FINAL TREATMENT CAN BE APPLIED. IF THE DATE IS AFTER NOVEMBER 1 AND IF THE EXPOSED AREA HAS BEEN LOADED, FINAL GRADED WITH A UNIFORM SURFACE, THEN THE AREA MAY BE DORMANT SEEDED AT A RATE OF 3 TIMES HIGHER THAN SPECIFIED FOR PERMANENT SEED AND THEN MULCHED. IF DORMANT SEEDING IS USED, ALL DISTURBED AREAS SHALL RECEIVE 4 INCHES OF LOAM AND SEED AT AN APPLICATION RATE OF 5 LBS PER 1,000 S.F. ALL AREAS INSUFFICIENTLY VEGETATED (LESS THAN 75%) IN THE SPRING SHALL BE REVEGETATED.

#### 6. OVER-WINTER STABILIZATION OF DITCHES AND CHANNELS

ALL STONE-LINED DITCHES AND CHANNELS MUST BE CONSTRUCTED BY NOVEMBER 1. ALL GRASS-LINED DITCHES AND CHANNELS MUST BE CONSTRUCTED AND STABILIZED BY SEPTEMBER 1. IF A GRASS-LINED DITCH OR CHANNEL IS STABILIZED BY SEPTEMBER 1, THEN EITHER A SOIL LINING SHALL BE INSTALLED PRIOR TO OCTOBER 1 OR THE DITCH MUST BE LINED WITH STONE RIPRAP BACKED BY AN APPROPRIATE GRAVEL BED OR GEOTEXTILE PRIOR TO NOVEMBER 1.

#### 7. OVER-WINTER STABILIZATION OF DISTURBED SLOPES

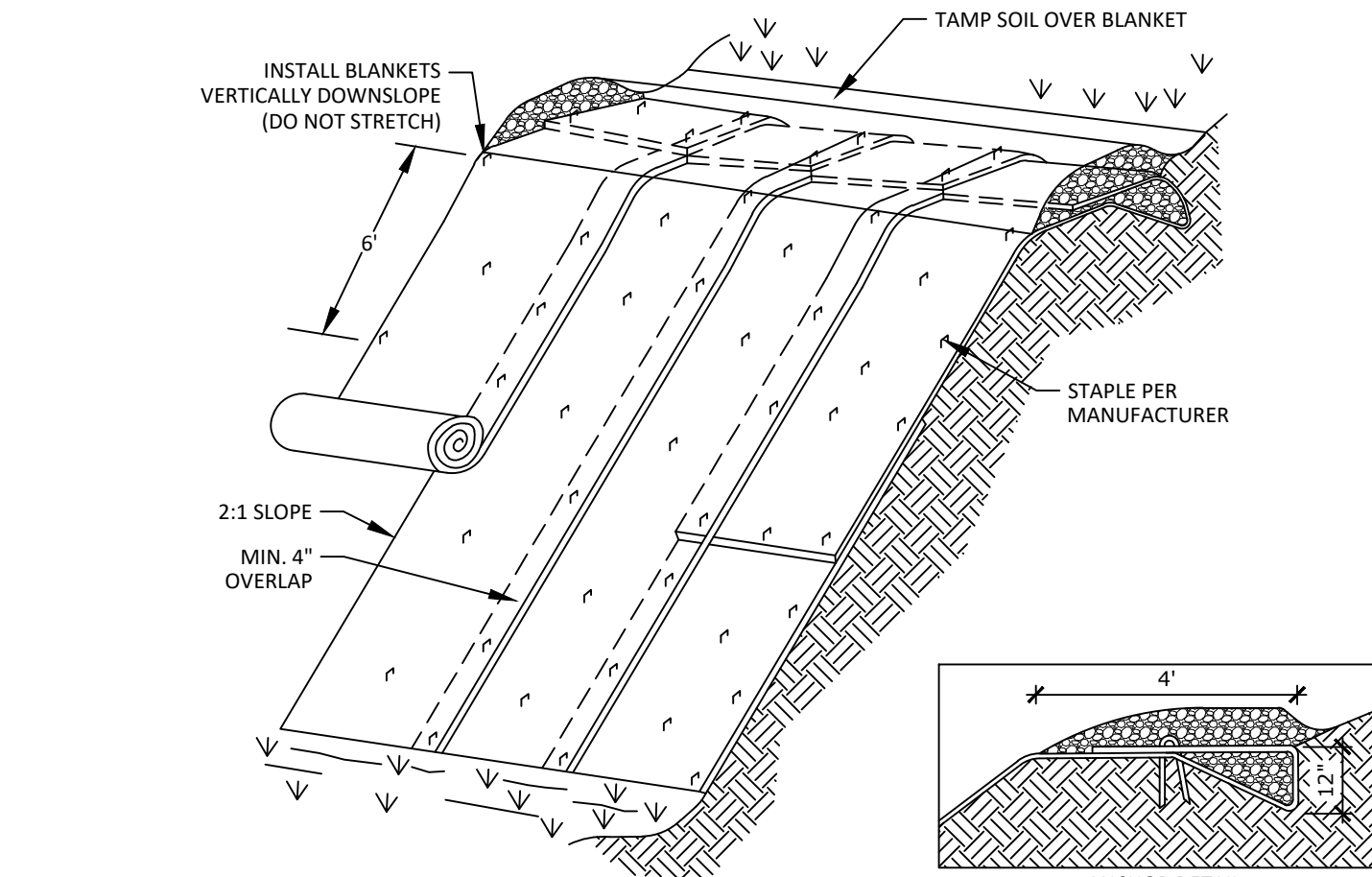
ALL STONE-COVERED SLOPES MUST BE CONSTRUCTED AND STABILIZED BY NOVEMBER 15. ALL SLOPES TO BE VEGETATED MUST BE SEEDED AND MULCHED BY SEPTEMBER 1. ALL AREAS HAVING A GRADE STEEPER THAN 8% SHALL BE CONSIDERED A SLOPE. IF A SLOPE TO BE VEGETATED IS NOT STABILIZED BY SEPTEMBER 1, THEN THE SLOPE SHALL EITHER BE STABILIZED WITH TEMPORARY VEGETATION AND EROSION CONTROL MATS BY OCTOBER 1, SOD BY OCTOBER 1, EROSION CONTROL MIX BY NOVEMBER 1 OR STONE RIPRAP BY NOVEMBER 15. SEE APPLICABLE SECTIONS UNDER EROSION AND SEDIMENTATION CONTROL NOTES FOR PROPER INSTALLATION METHODS.

#### 8. OVER-WINTER STABILIZATION OF DISTURBED SOILS

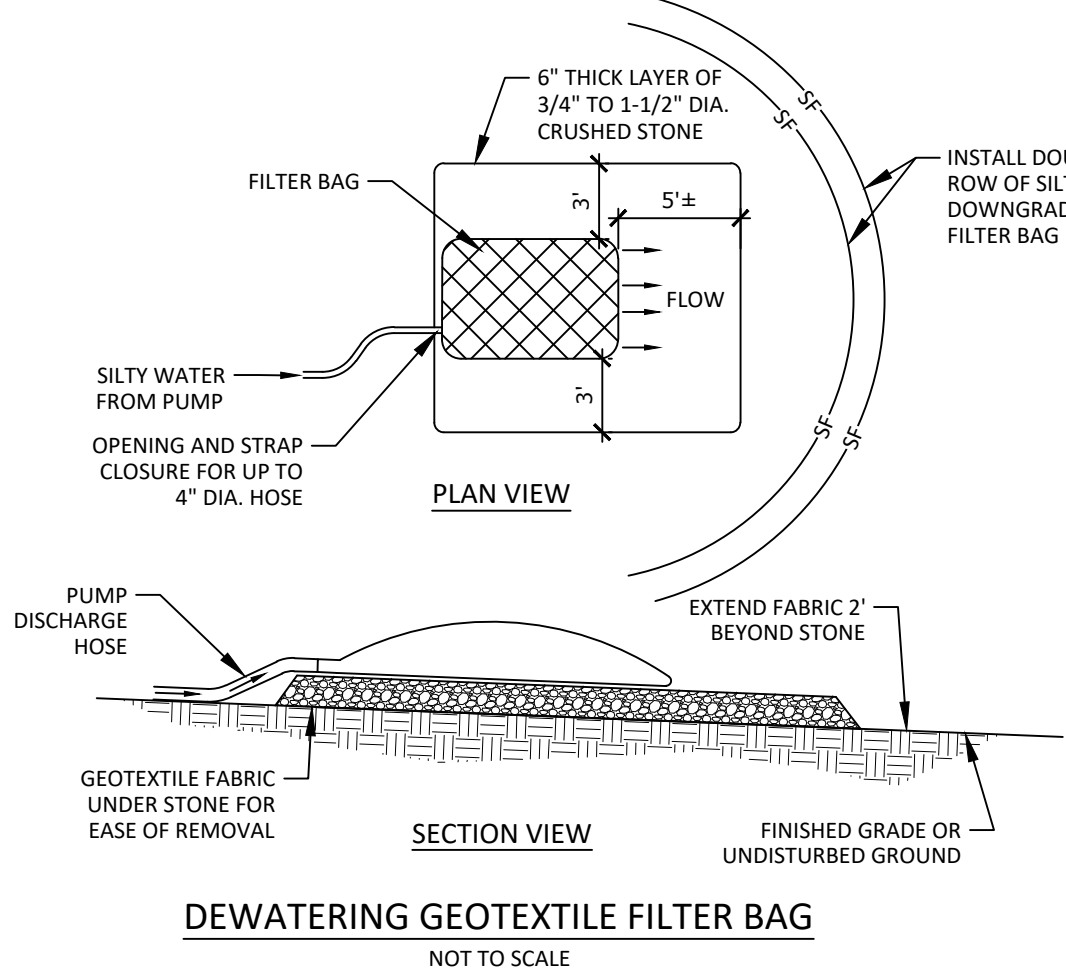
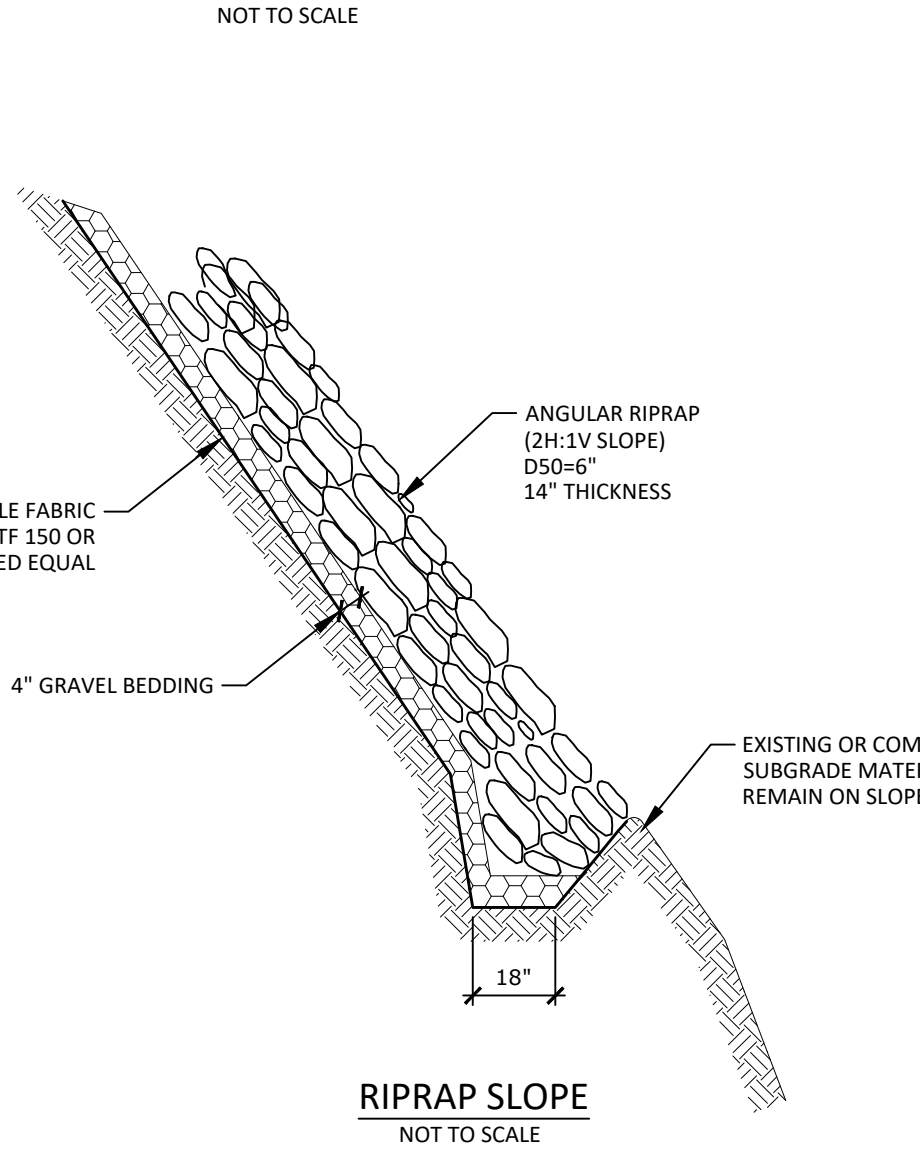
BY SEPTEMBER 15, ALL DISTURBED SOILS ON AREAS HAVING A SLOPE LESS THAN 15% MUST BE SEEDED AND MULCHED. IF THE DISTURBED AREAS ARE NOT STABILIZED BY THIS DATE, THEN THE AREA SHALL EITHER BE STABILIZED WITH TEMPORARY VEGETATION BY OCTOBER 1, SOD BY OCTOBER 1, OR MULCH BY NOVEMBER 15. SEE APPLICABLE SECTIONS UNDER EROSION AND SEDIMENTATION CONTROL NOTES FOR PROPER INSTALLATION METHODS.

#### 9. MAINTENANCE

MAINTENANCE MEASURES SHALL BE APPLIED AS NEEDED DURING THE ENTIRE CONSTRUCTION SEASON. AFTER EACH RAINFALL, SNOW STORM, PERIOD OF THAWING AND RUNOFF AND AT LAST ONCE A WEEK, THE SITE CONTRACTOR SHALL PERFORM A VISUAL INSPECTION OF ALL INSTALLED EROSION CONTROL MEASURES AND PERFORM REPAIRS AS NEEDED TO INSURE THEIR CONTINUOUS FUNCTION. FOLLOWING THE TEMPORARY AND/OR FINAL SEEDING AND MULCHING, THE CONTRACTOR SHALL, IN THE SPRING, INSPECT AND REPAIR ANY DAMAGES AND/OR BARE SPOTS. AN ESTABLISHED VEGETATIVE COVER MEANS A MINIMUM OF 90% OF AREAS VEGETATED WITH VIGOROUS GROWTH.

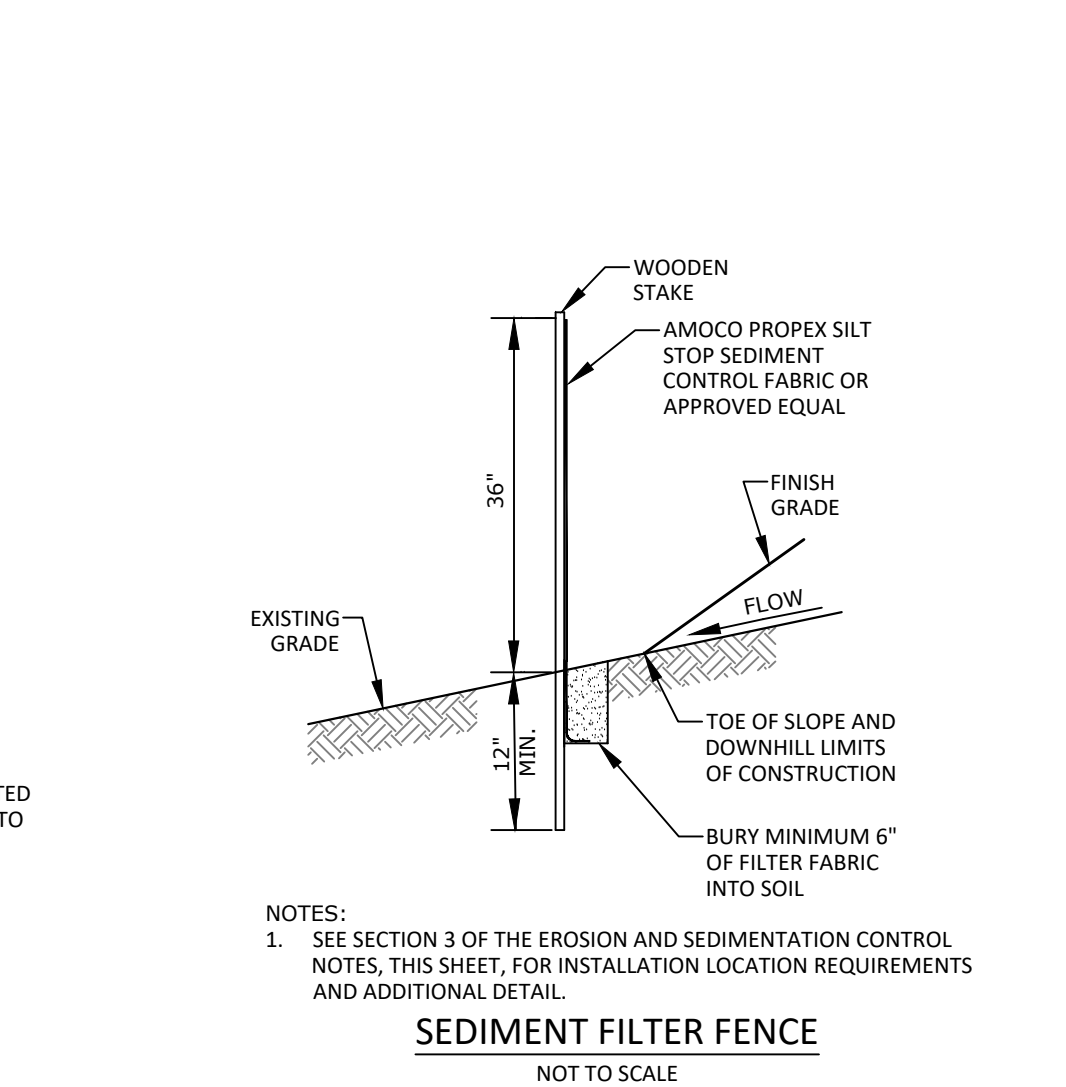


## EROSION CONTROL BLANKET



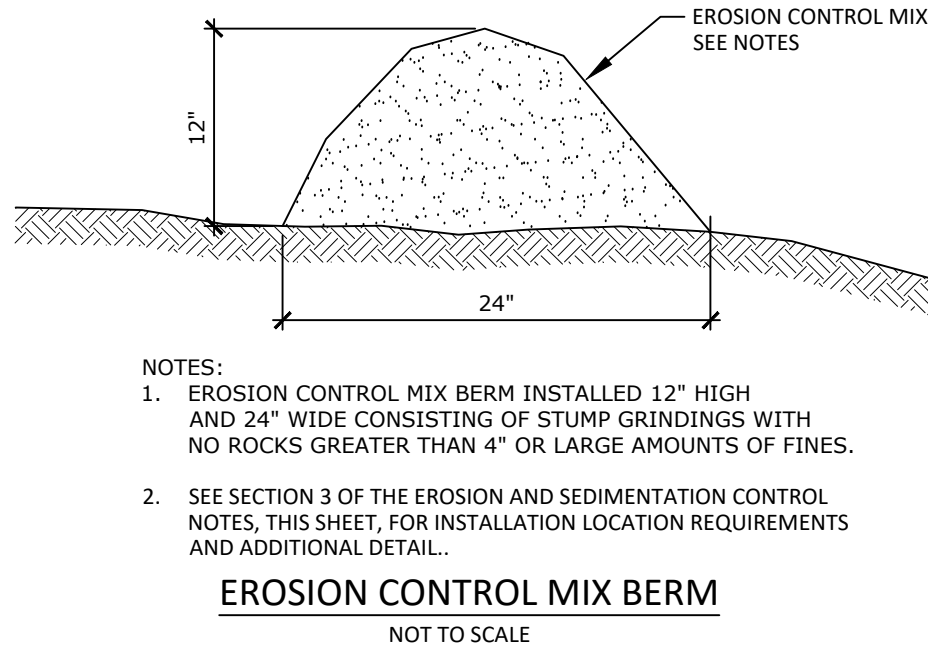
## DEWATERING GEOTEXTILE FILTER BAG

NOT TO SCALE



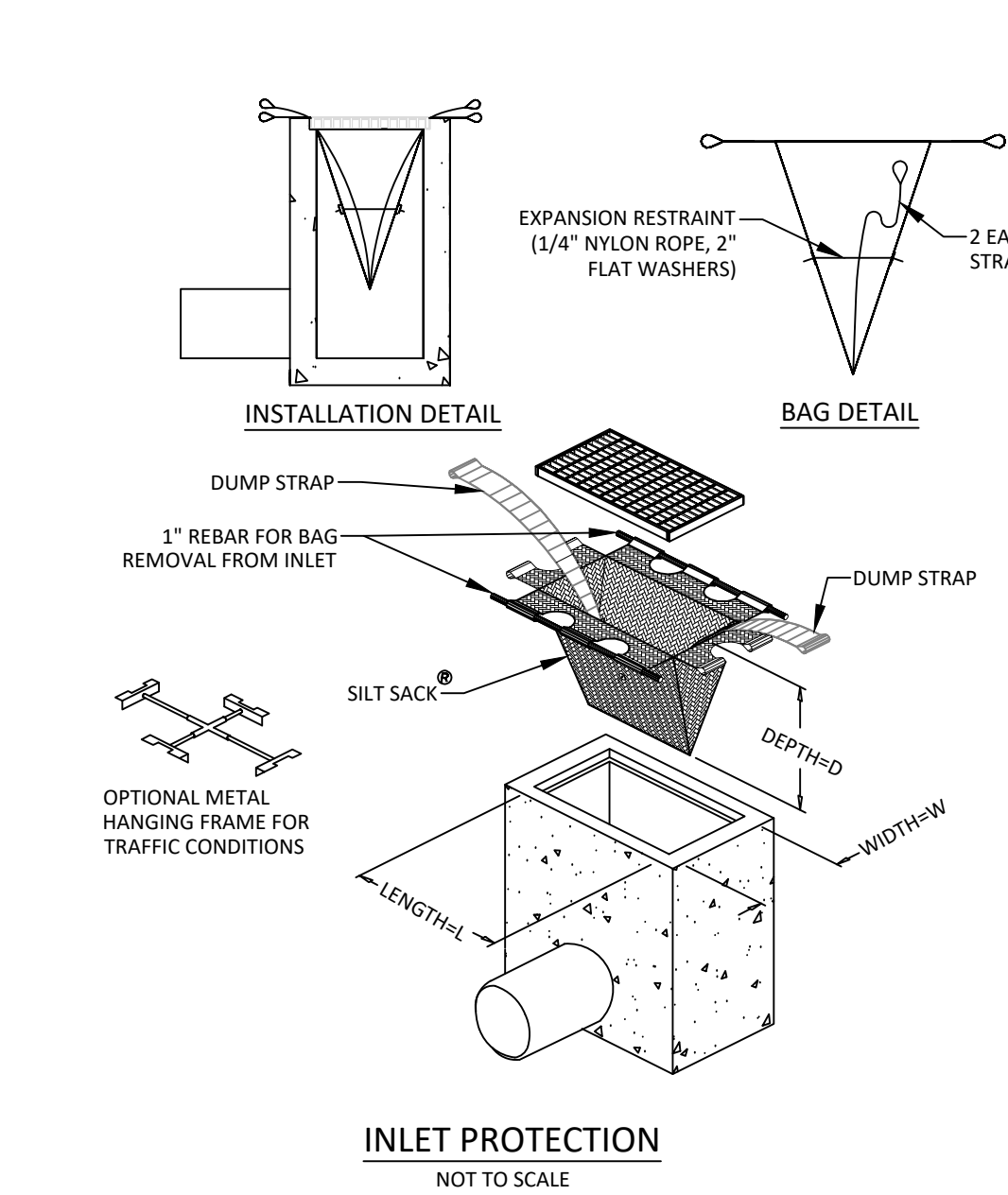
## SEDIMENT FILTER FENCE

NOT TO SCALE



## EROSION CONTROL MIX BERM

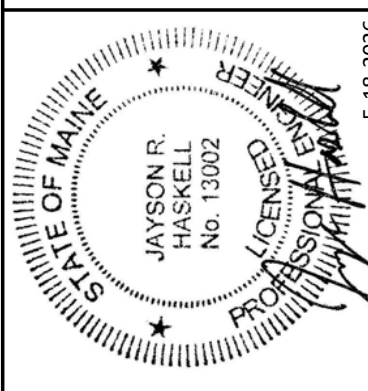
NOT TO SCALE



## INLET PROTECTION

NOT TO SCALE

PERMITTING PLAN  
NOT FOR  
CONSTRUCTION

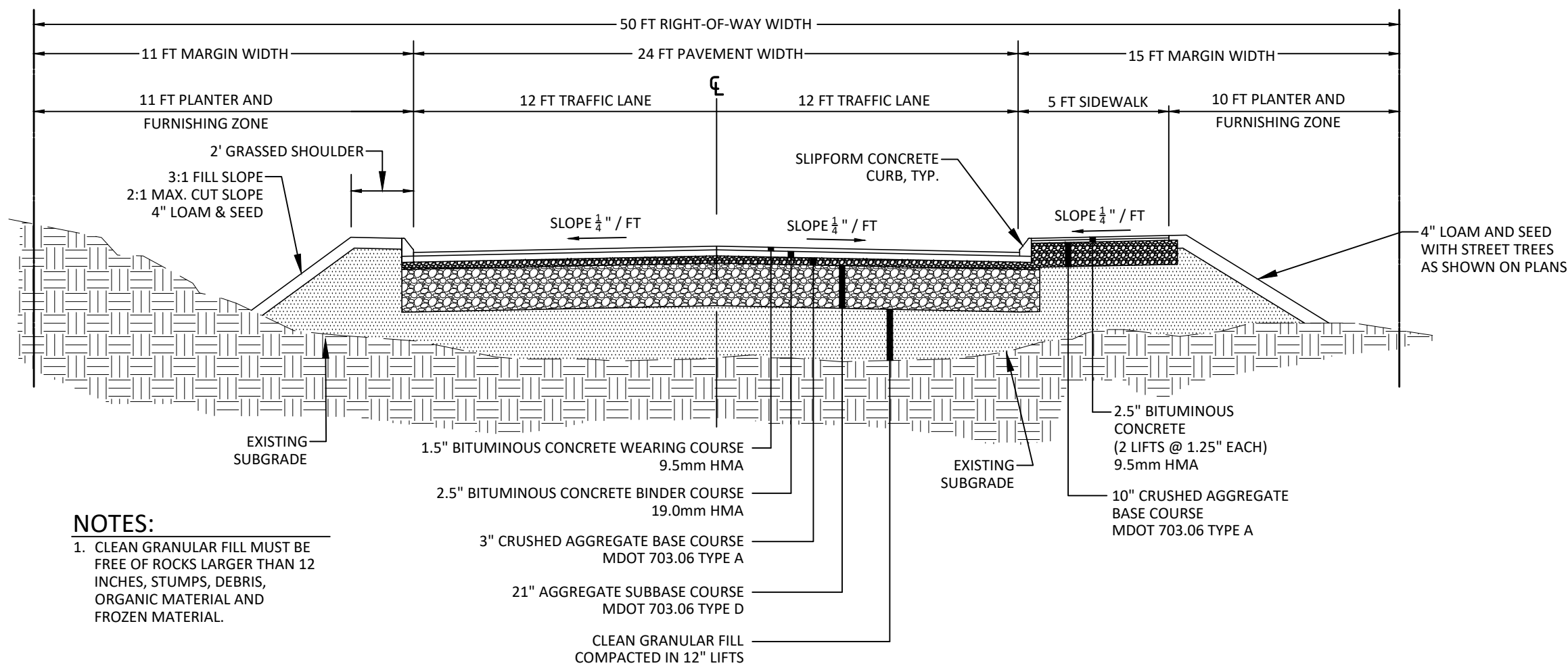


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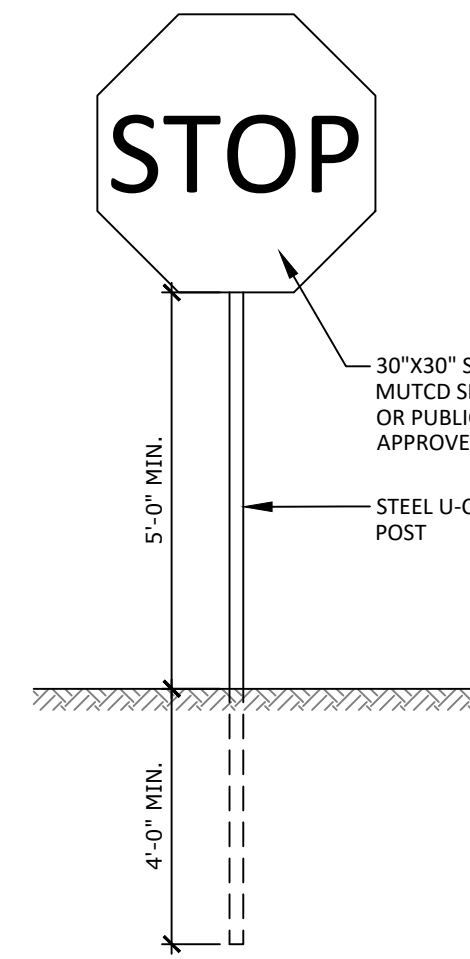
REV.	DATE	BY	DESCRIPTION
A	9-25-24	JRH	ISSUED FOR MDEP PERMIT REVIEW
B	7-7-25	JRH	ISSUED FOR TOWN REVIEW
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DETAILS  
TURNING LEAF HEIGHTS  
TURNING LEAF DRIVE & DRIVE IN LANE  
WINDHAM, MAINE  
FOR:  
GATEWAY DEVELOPMENT, LLC  
4 COWBOY AVENUE, SUITE 12  
WINDHAM, ME 04962

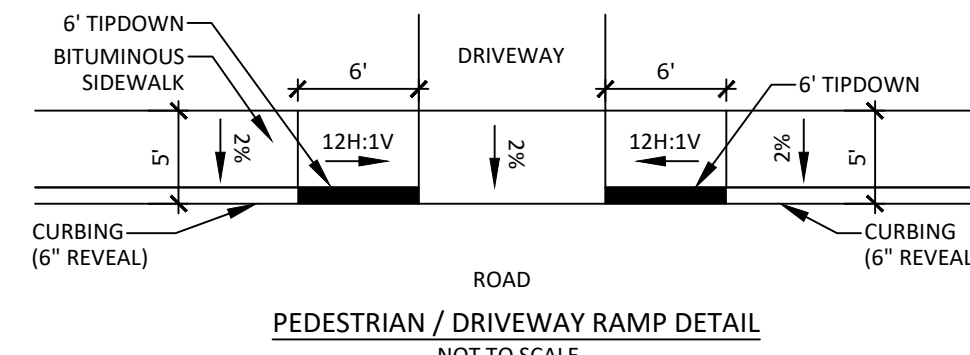
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JOB NUMBER:  
AS NOTED  
SCALE:  
5-18-2026  
DATE:  
SHEET 16 OF 19  
D-1



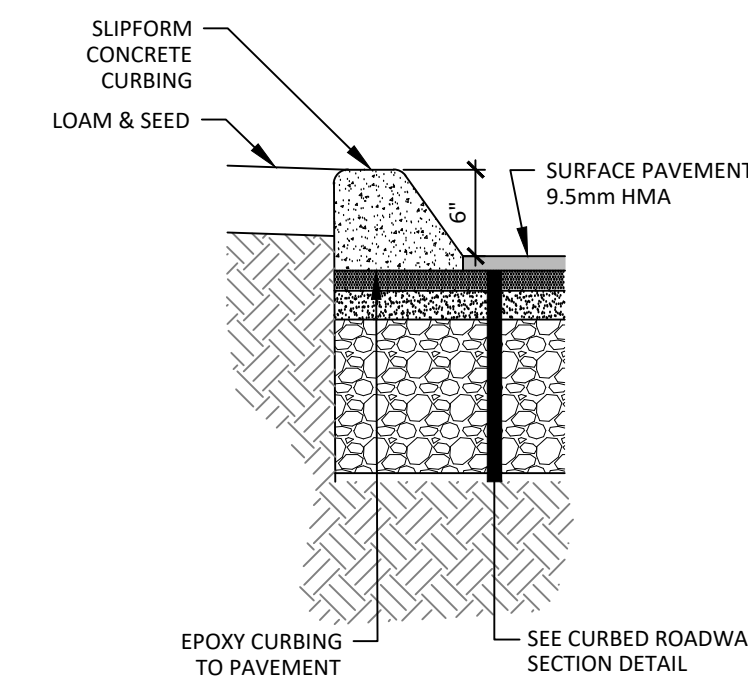
TYPICAL ROADWAY SECTION  
NOT TO SCALE



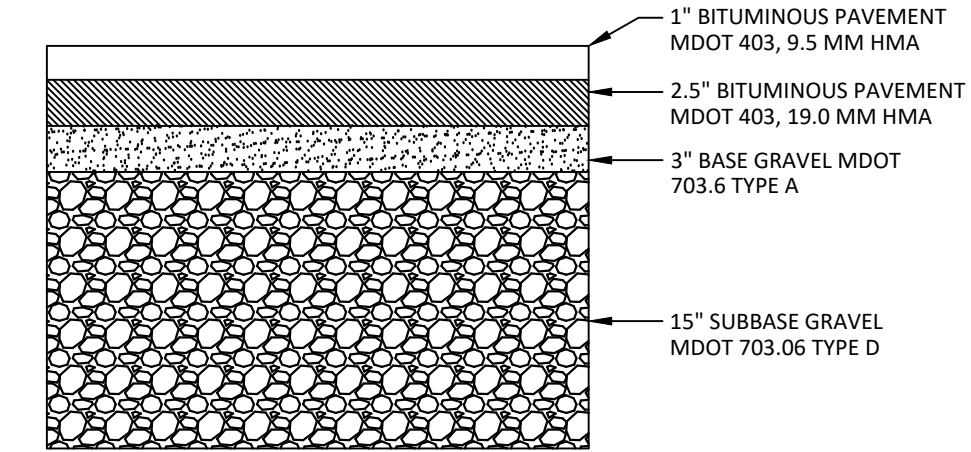
STOP SIGN INSTALLATION  
NOT TO SCALE



PEDESTRIAN / DRIVEWAY RAMP DETAIL  
NOT TO SCALE



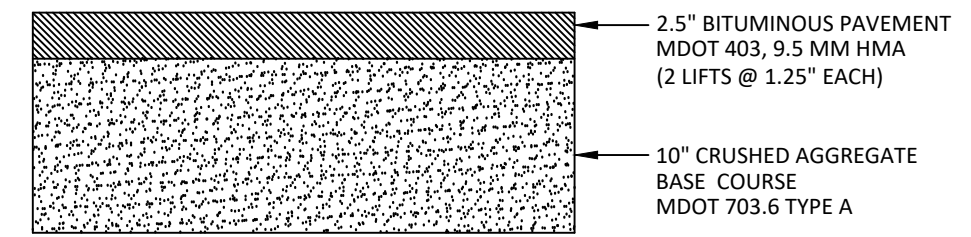
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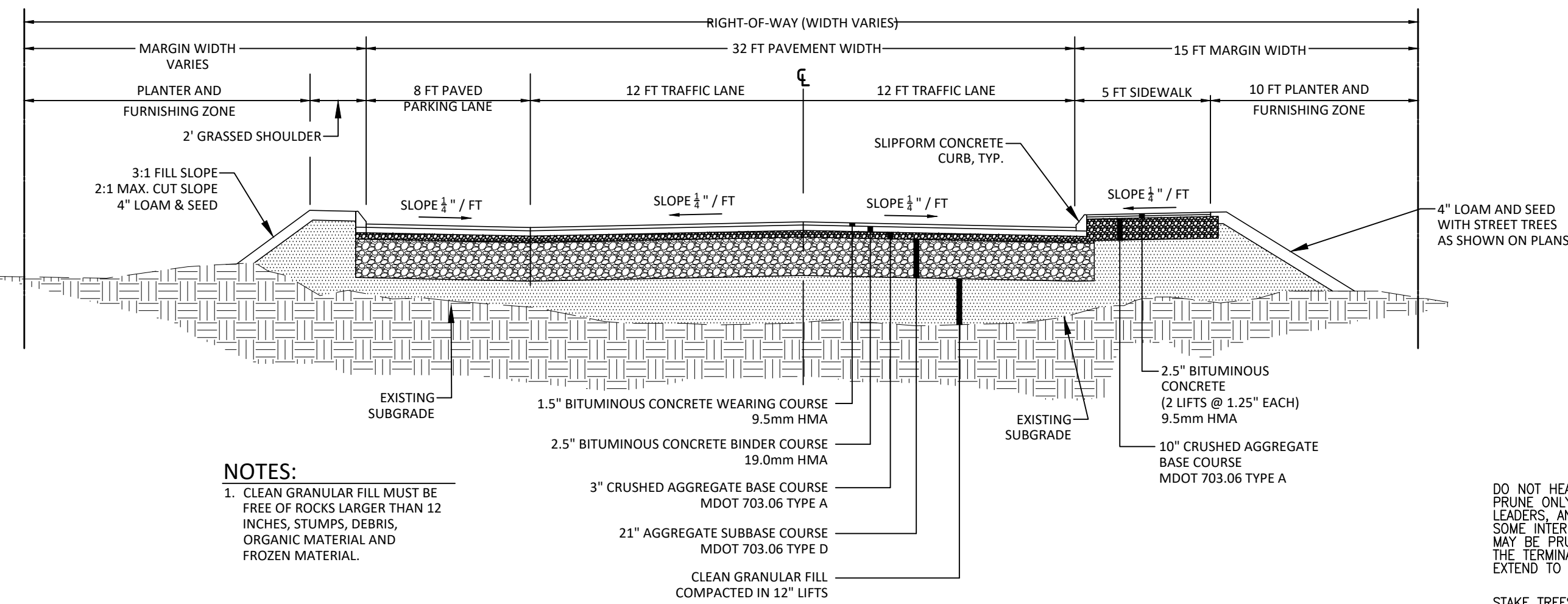
TYPICAL UNIT DRIVEWAY SECTION  
NOT TO SCALE

**NOTES:**

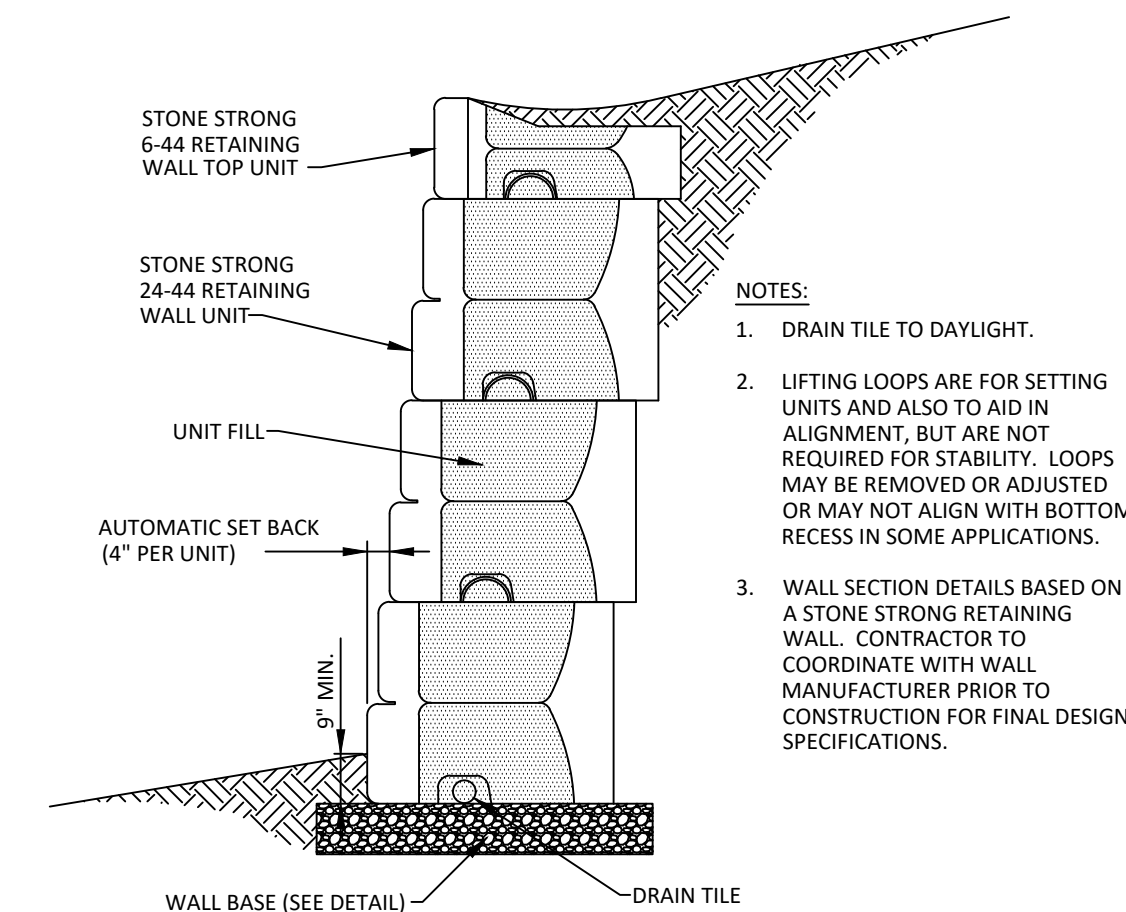
- CLEAN GRANULAR FILL MUST BE FREE OF ROCKS LARGER THAN 12 INCHES, STUMPS, DEBRIS, ORGANIC MATERIAL AND FROZEN MATERIAL.



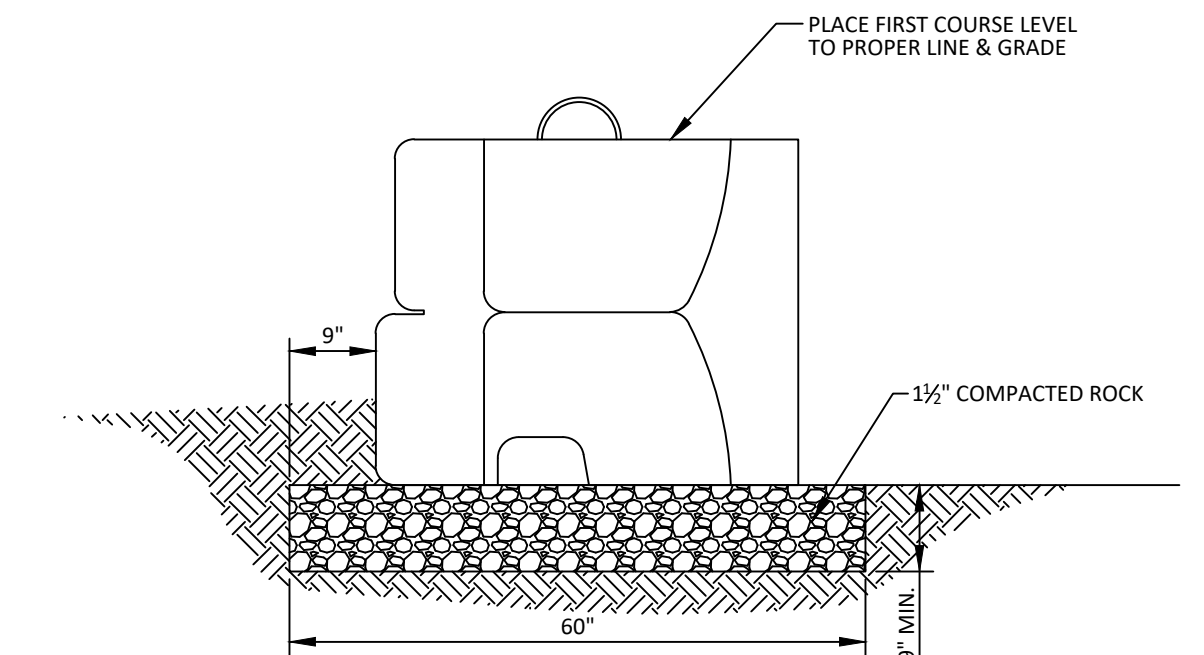
TYPICAL PAVED WALKING PATH OR SIDEWALK SECTION  
NOT TO SCALE



TURNING LEAF DRIVE TYPICAL ROADWAY SECTION WITH ON-STREET PARKING ONE SIDE  
NOT TO SCALE



GRAVITY WALL CROSS SECTION  
NOT TO SCALE



WALL BASE DETAIL  
NOT TO SCALE

DO NOT HEAVILY PRUNE THE TREE AT PLANTING. PRUNE ONLY CROSSOVER LIMBS, DO-DOMINANT LEADERS, AND BROKEN OR DEAD BRANCHES. SOME INTERIOR TWIGS AND LATERAL BRANCHES MAY BE TRIMMED, HOWEVER, DO NOT REMOVE THE TERMINAL BUDS OF BRANCHES THAT EXTEND TO THE EDGE OF THE CROWN.

STAKE TREES ONLY UPON THE APPROVAL OF THE LANDSCAPE ARCHITECT.

WRAP TREE TRUNKS ONLY UPON THE APPROVAL OF THE LANDSCAPE ARCHITECT.

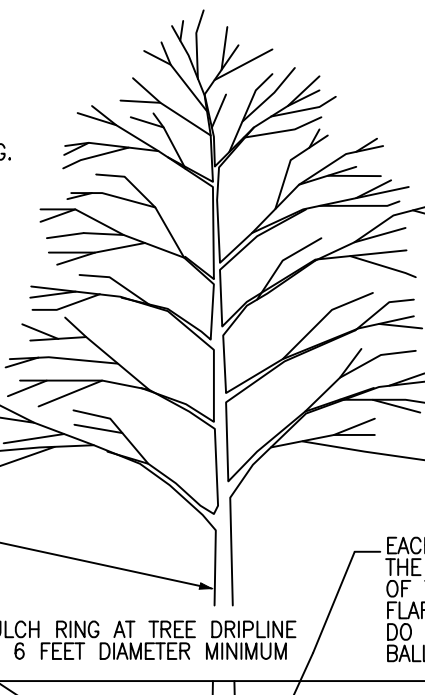
MARK THE NORTH SIDE OF THE TREE IN THE NURSERY, AND ROTATE TREE TO FACE NORTH AT THE SITE WHEN EVER POSSIBLE.

SET TOP OF ROOT BALL FLUSH TO GRADE OR 1-2 INCHES HIGHER IN SLOWLY DRAINING SOILS.

MINIMUM 8 INCHES BARE ROOTBALL MEASURED FROM TRUNK CROWN

3 INCHES MULCH, DO NOT PLACE MULCH IN CONTACT WITH TREE TRUNK.

NOTE: FOR DIMENSIONS OF PLANTING AREAS, TYPES OF SOIL AMENDMENTS, OR SOIL REPLACEMENT, SEE LANDSCAPE NOTES AND/OR SPECS.



TREE INSTALLATION DETAIL - BALL & BURLAP  
NOT TO SCALE

EACH TREE MUST BE PLANTED SUCH THAT THE TRUNK FLARE IS VISIBLE AT THE TOP OF THE ROOT BALL. TREES WHERE THE TRUNK FLARE IS NOT VISIBLE SHALL BE REJECTED. DO NOT COVER THE TOP OF THE ROOT BALL WITH SOIL.

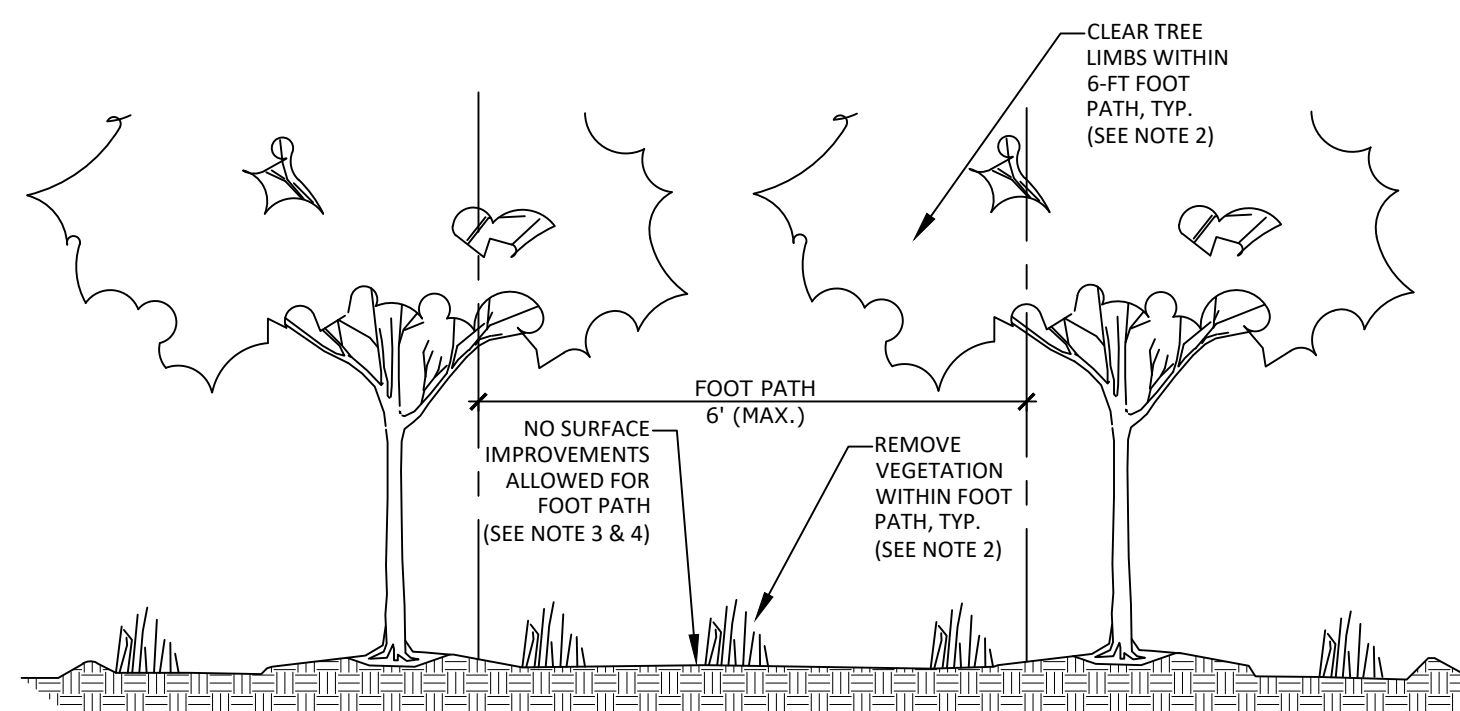
MINIMUM 3 INCHES HIGH EARTH SAUCER BEYOND EDGE OF ROOT BALL.

REMOVE ALL TWINE, ROPE AND WIRE, AND BURLAP FROM TOP HALF OF ROOT BALL

IF PLANT IS SHIPPED WITH A WIRE BASKET AROUND THE ROOT BALL, CUT AND REMOVE AS MUCH OF WIRE CAGE AS POSSIBLE WITHOUT DAMAGING ROOTBALL INTEGRITY

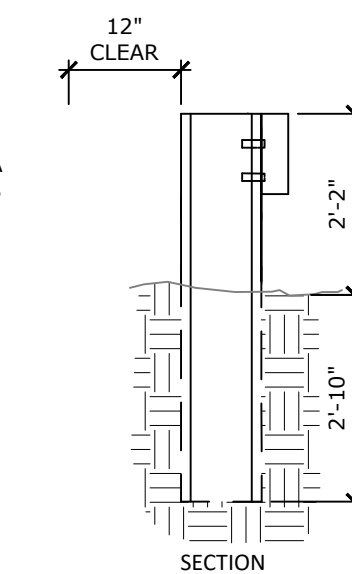
PLACE ROOT BALL ON UNEXCAVATED OR TAMPED SOIL.

TAMP SOIL AROUND ROOT BALL BASE FIRMLY WITH FOOT PRESSURE SO THAT ROOT BALL DOES NOT SHIFT.

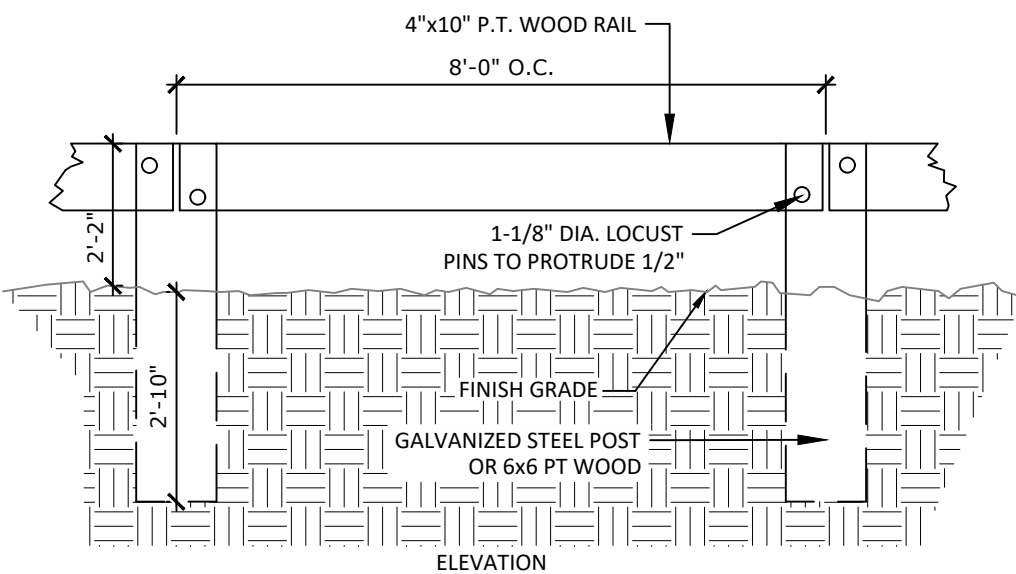


FOOT PATH DETAIL

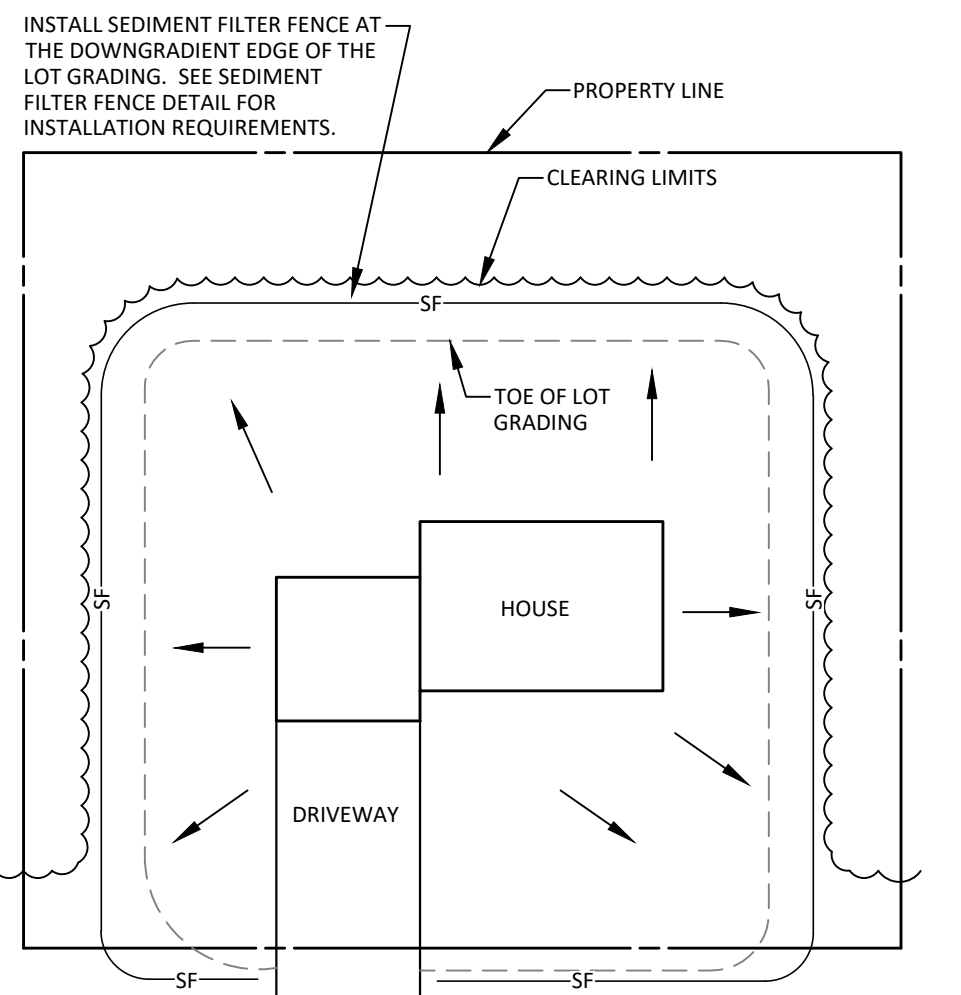
- NOTES:**
- PROPOSED FOOT PATH THROUGH THE OPEN SPACE A SHALL BE LOCATED SUCH THAT NO TREE CLEARING IS REQUIRED.
  - CLEARING FOR FOOT PATH IS LIMITED TO REMOVING BRUSH AND VEGETATION, AND IF NECESSARY TRIMMING OF LIMBS ONLY.
  - FOOT PATH SHALL NOT BE INSTALLED WITH ANY SURFACE IMPROVEMENTS OR EARTH DISTURBANCE (NO EXCAVATION OR PLACEMENT OF FILL, AND NO INSTALLATION OF GRAVEL/PAVEMENT).
  - IN THE EVENT NO ALTERNATE FOOT PATH LOCATION IS ACCEPTABLE, 4-FT WIDE (MAX.) STAIRS MAY BE INSTALLED WHERE SLOPES EXCEED 15%.



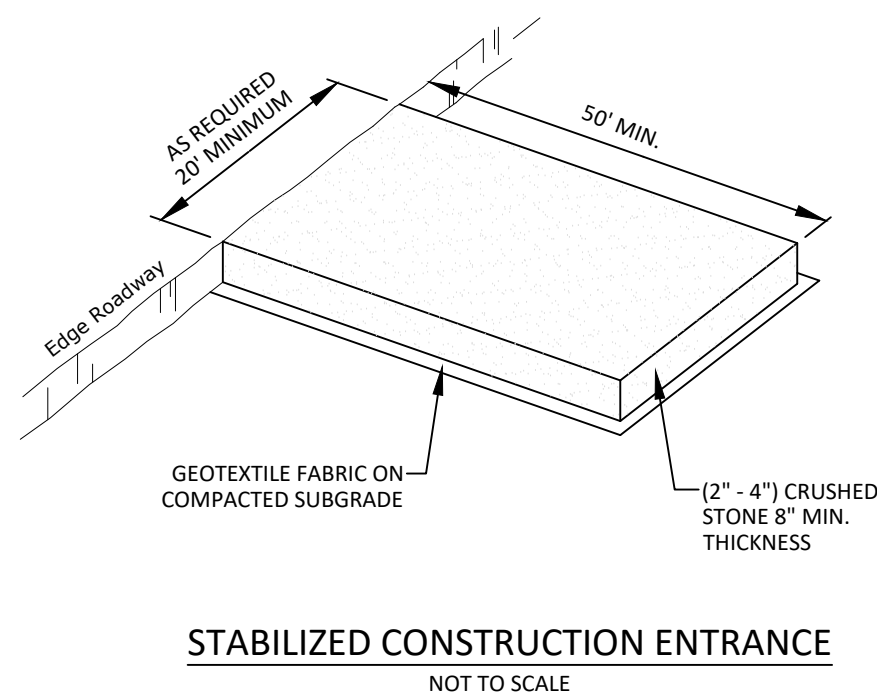
WOOD GUARDRAIL DETAIL  
NOT TO SCALE



WOOD GUARDRAIL DETAIL  
NOT TO SCALE

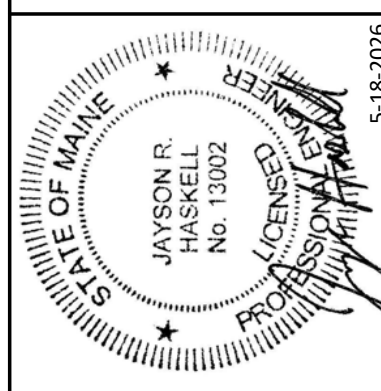


TYPICAL HOUSE LOT EROSION CONTROL DETAIL  
NOT TO SCALE



STABILIZED CONSTRUCTION ENTRANCE  
NOT TO SCALE

PERMITTING PLAN  
NOT FOR  
CONSTRUCTION

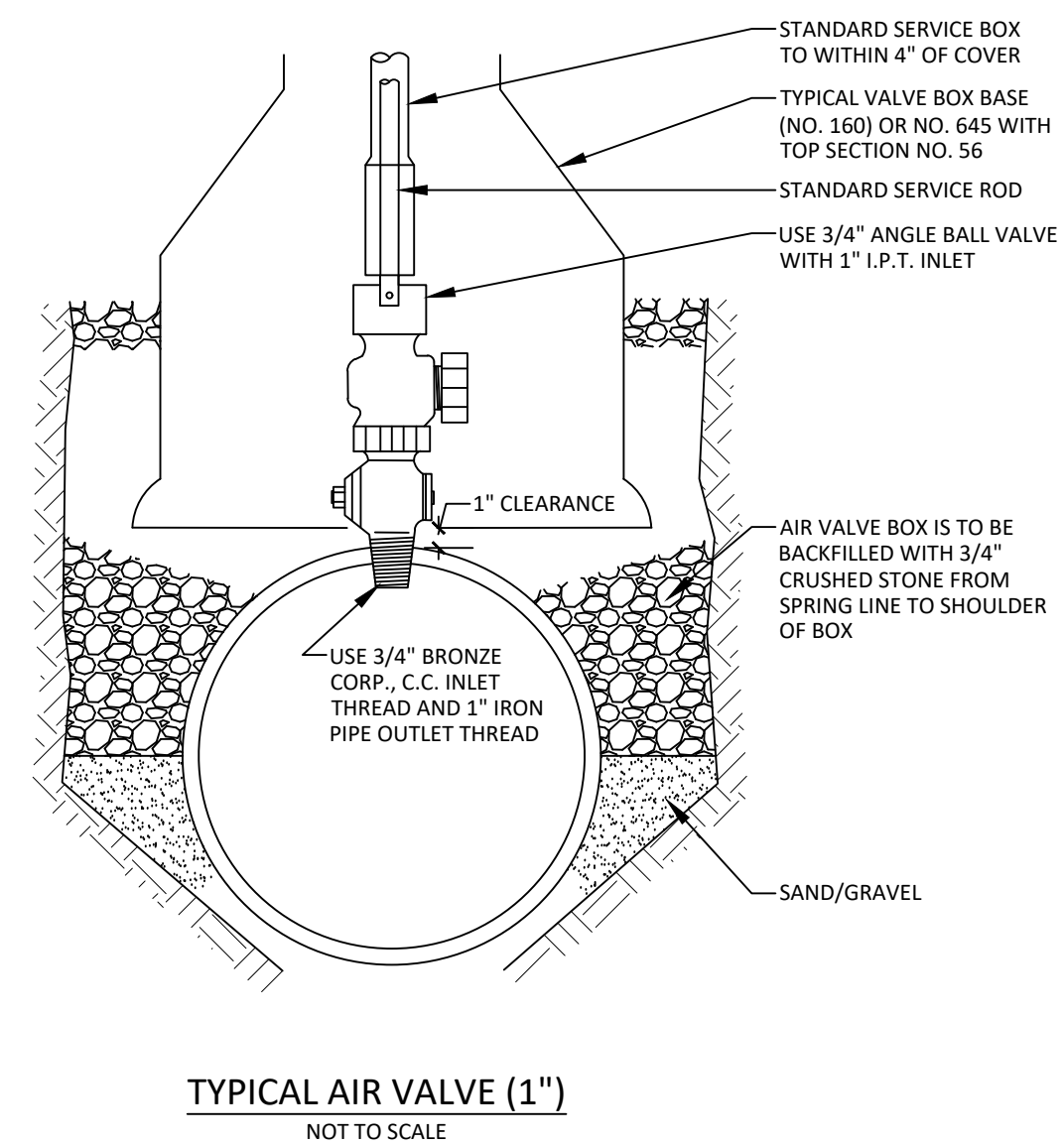
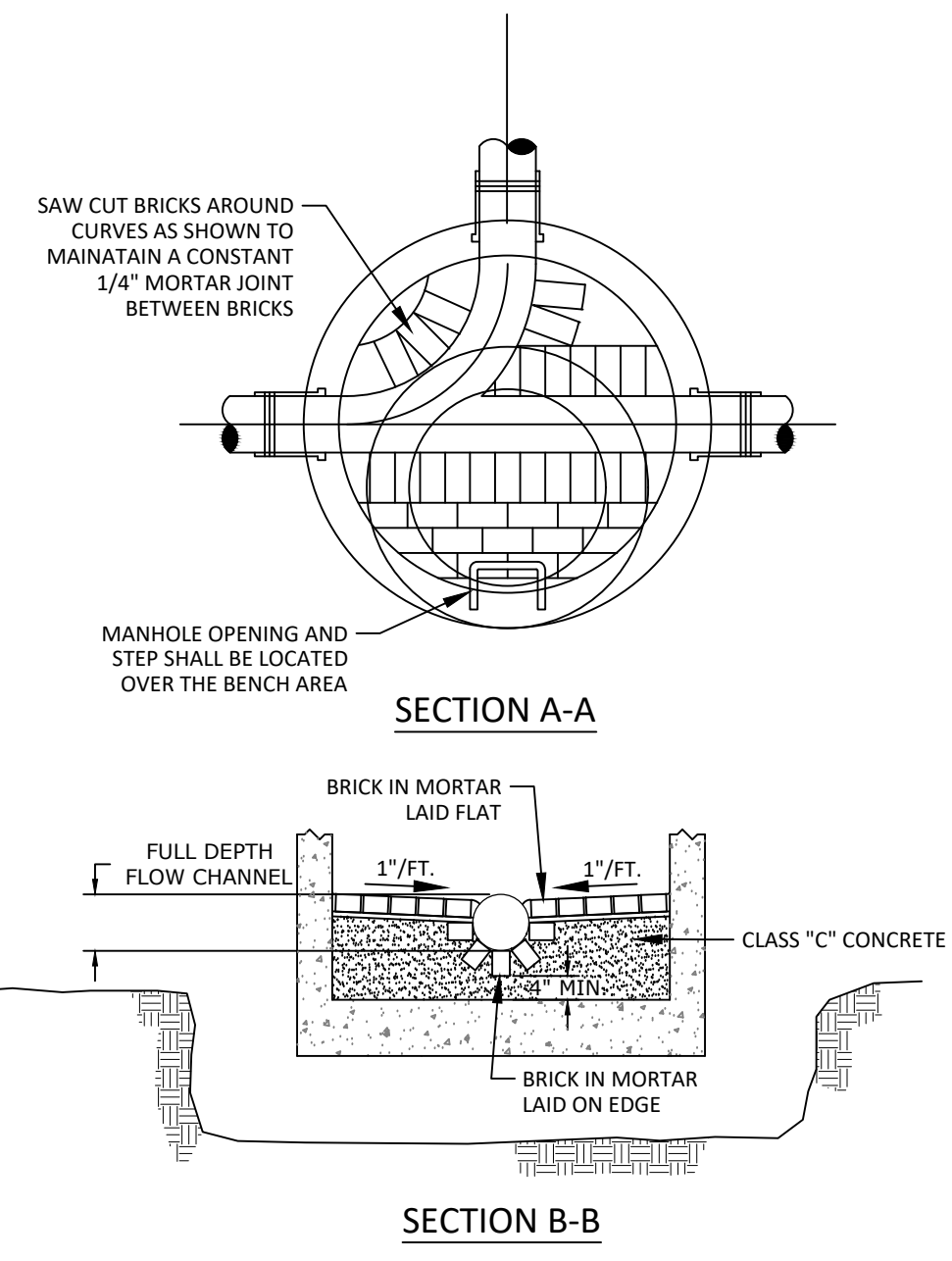
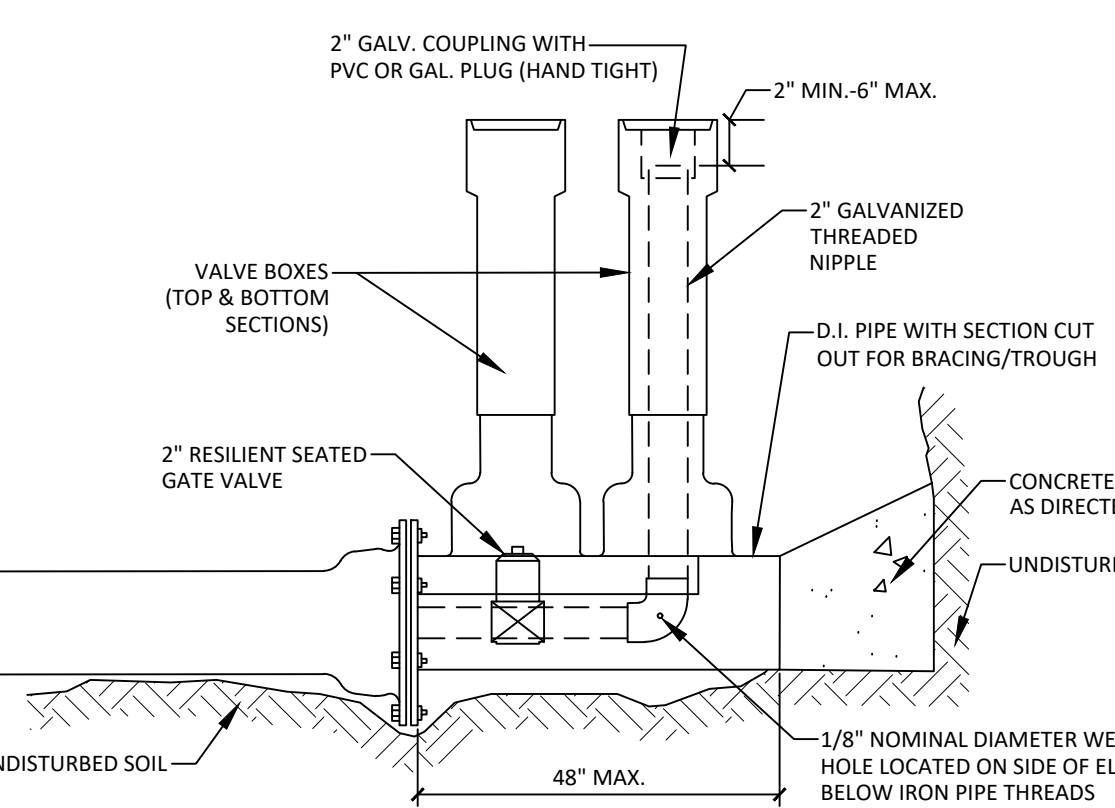
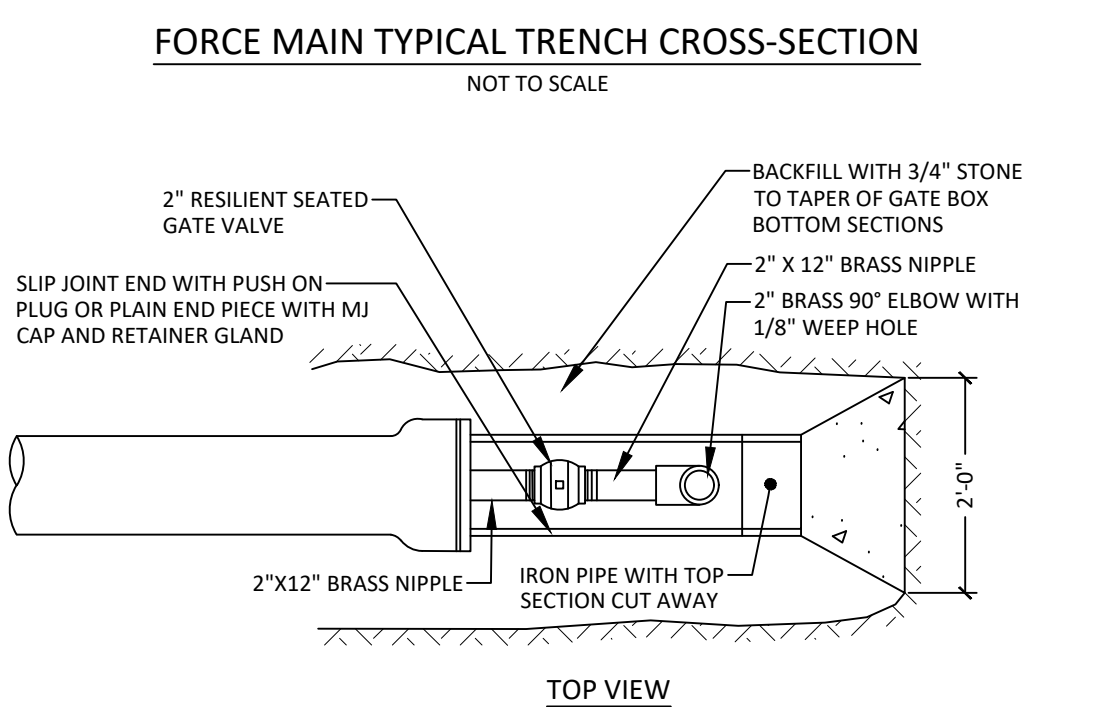
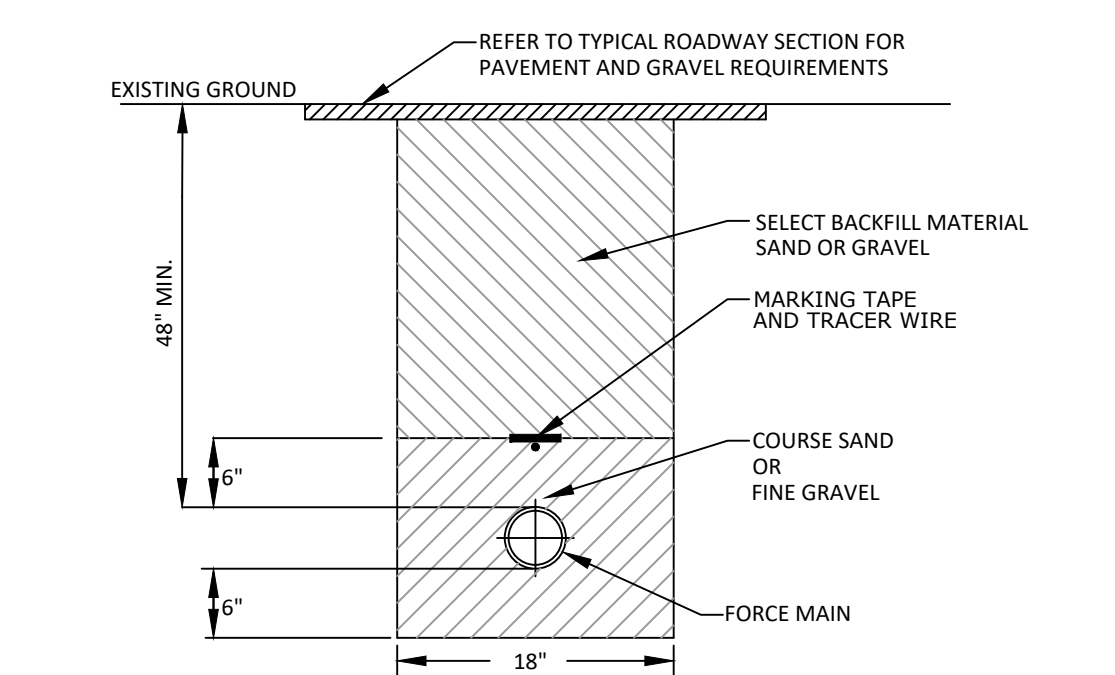
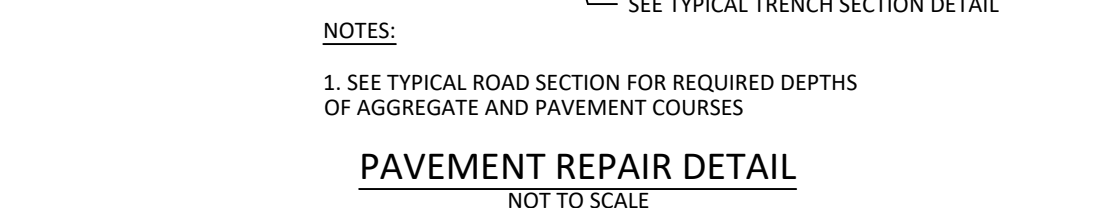
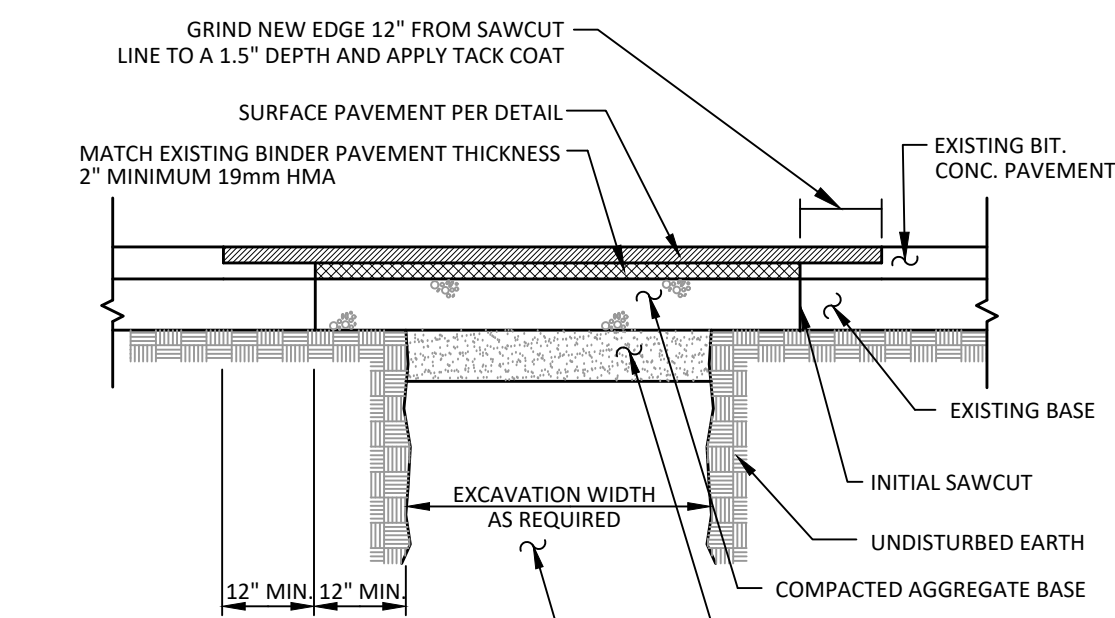
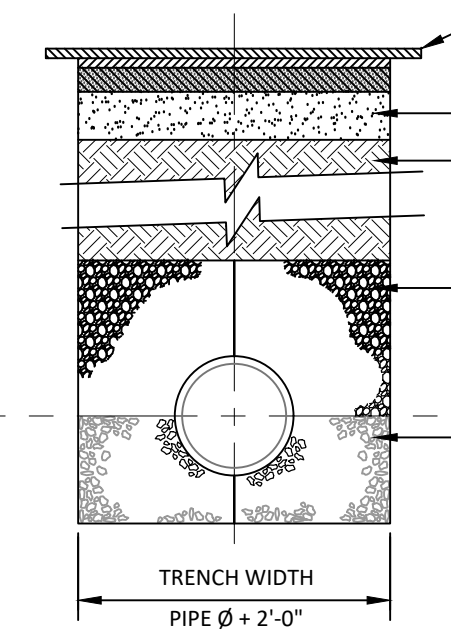


**DM ROMA**  
CONSULTING ENGINEERS  
P.O. BOX 1116  
WINDHAM, ME 04062  
(207) 591-5055

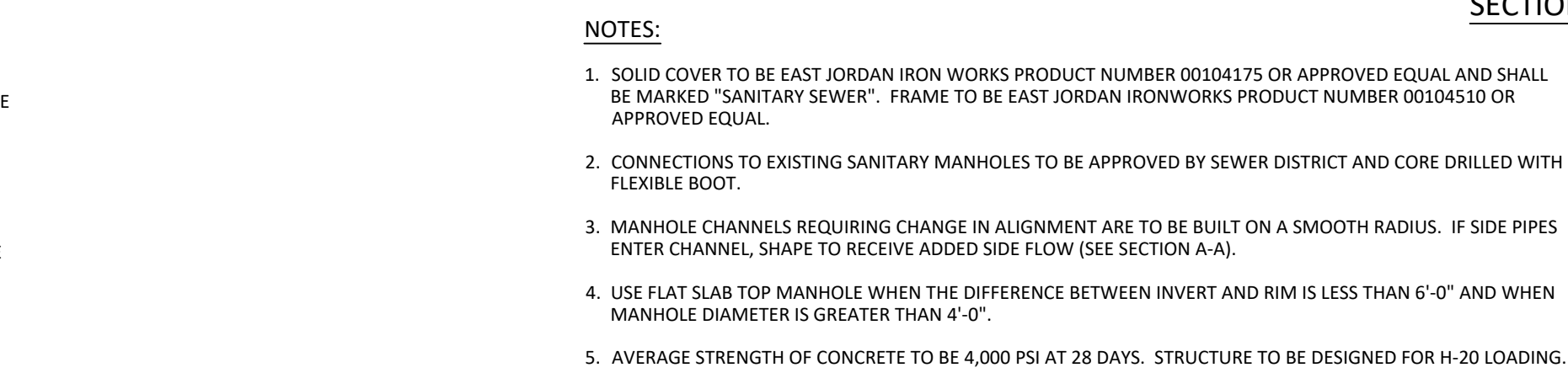
REV	DATE	BY	DESCRIPTION
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**DETAILS**  
TURNING LEAF HEIGHTS  
TURNING LEAF DRIVE & DRIVE IN LANE  
WINDHAM, MAINE  
FOR:  
GATEWAY DEVELOPMENT, LLC  
4 COMMANDERS AVENUE SUITE 12  
WINDHAM, ME 04062

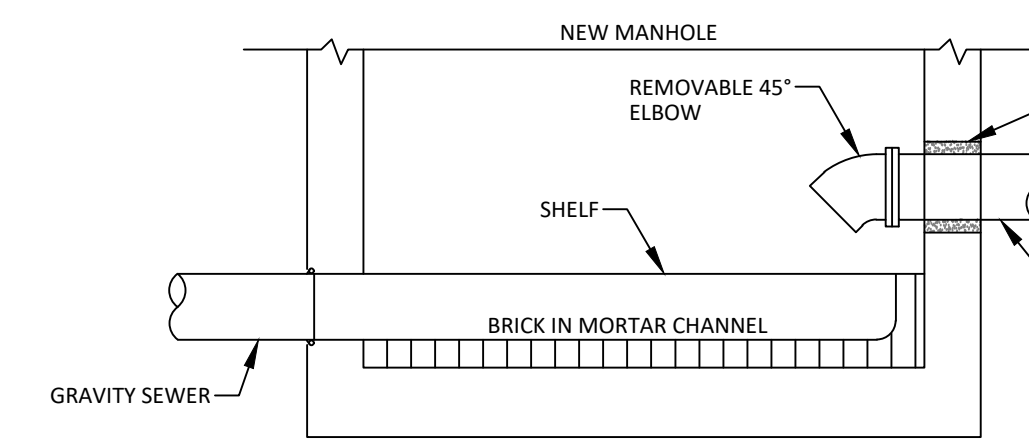
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SHEET 17 OF 19  
D-2



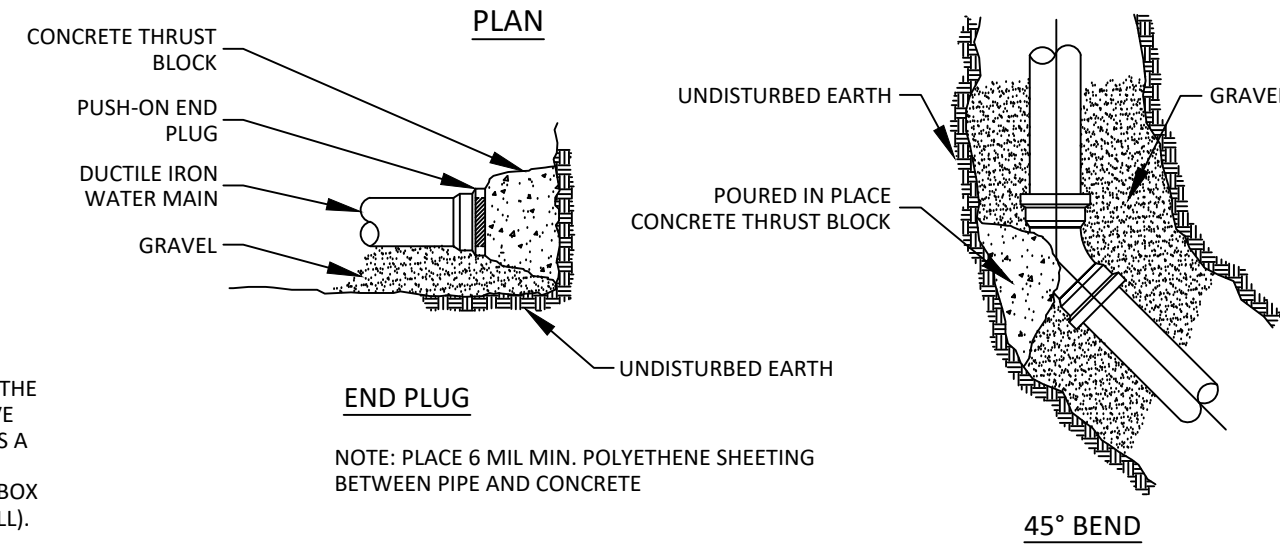
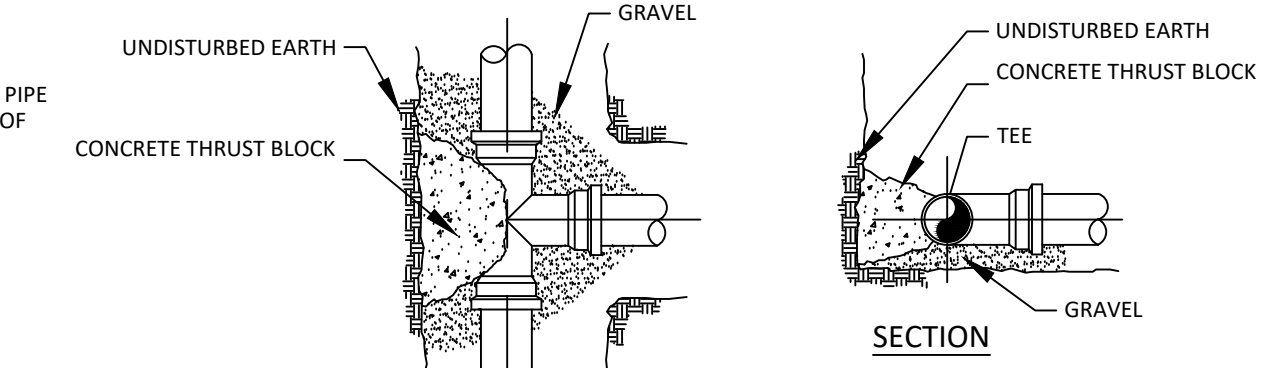
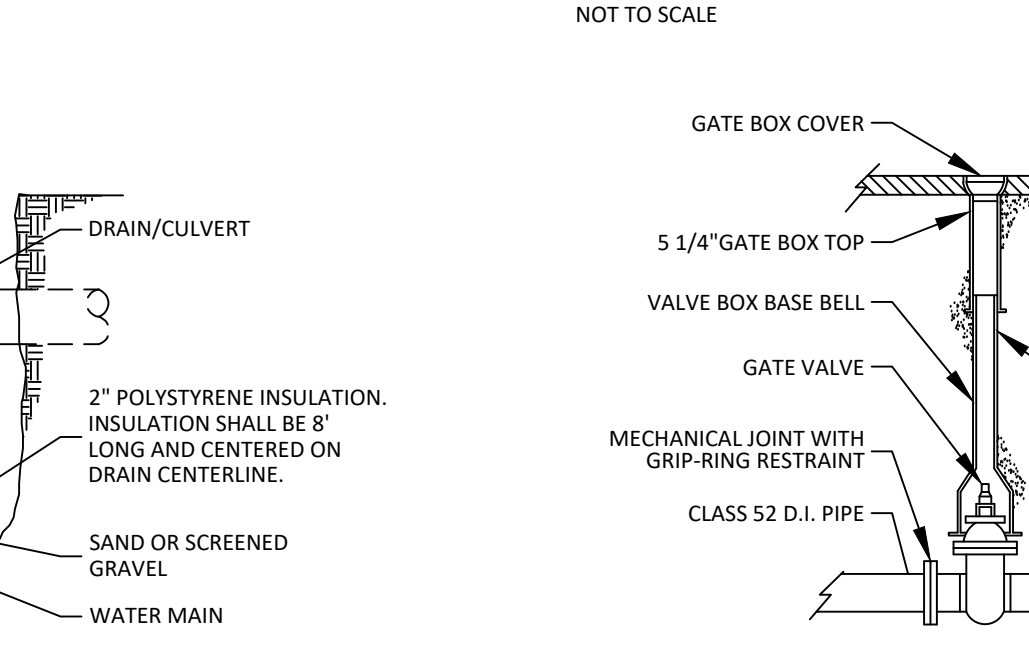
CONCRETE THRUST BLOCK SIZE REQUIREMENTS				
		SQ. FT. OF BEARING ON UNDISTURBED SOIL		
FITTINGS		90°BENDS	45°BENDS	TEES AND PLUGS
PIPE SIZE	6"	4.0	2.0	3.0
	8"	8.0	4.0	6.0
	12"	15	9	12
	16"	26	14	19
	20"	40	22	28



PRECAST CONCRETE SANITARY MANHOLE  
NOT TO SCALE

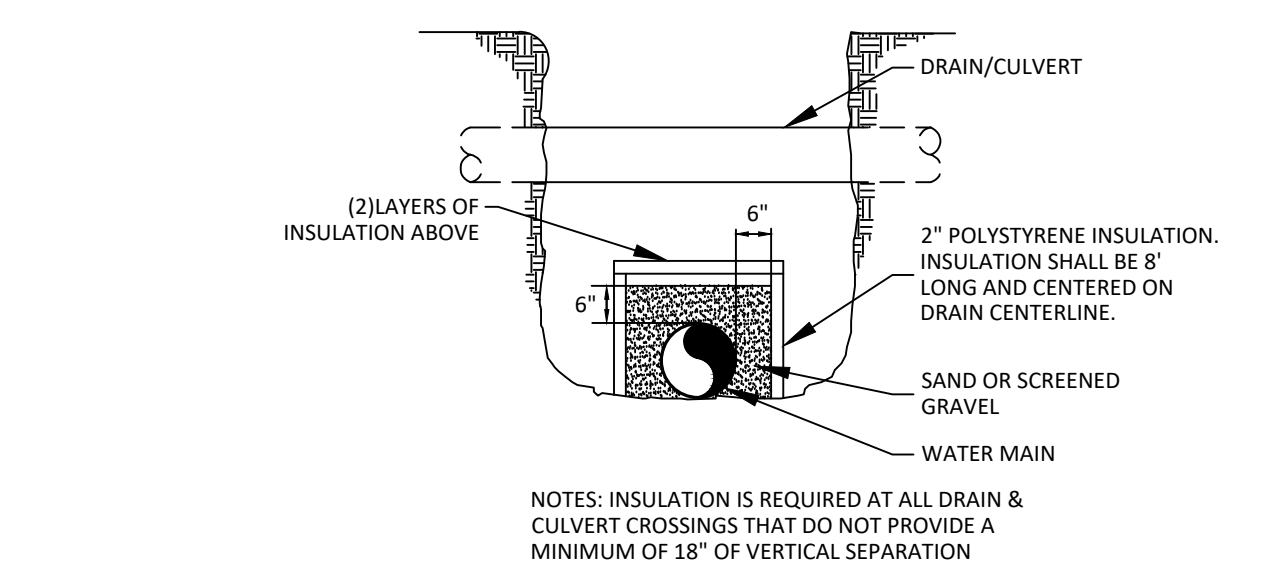


## FORCE MAIN TERMINOUS

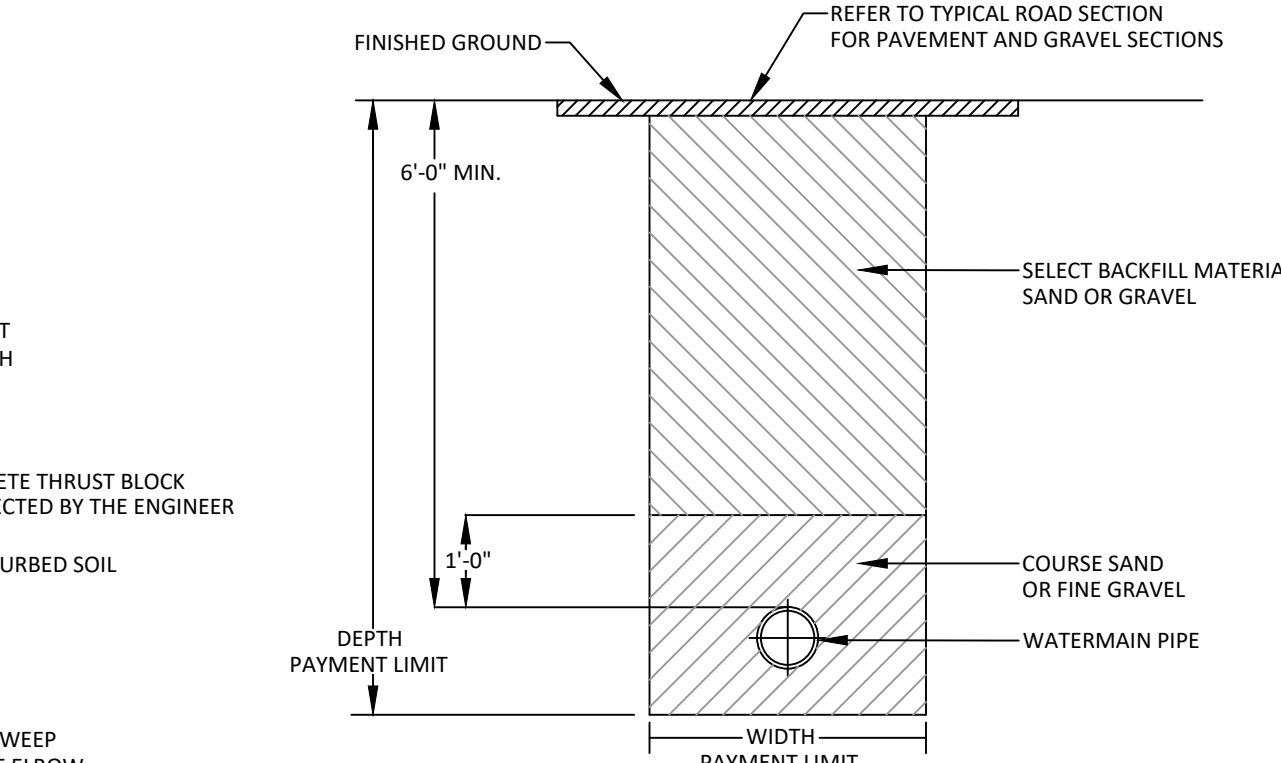


THRUST BLOCK DETAIL

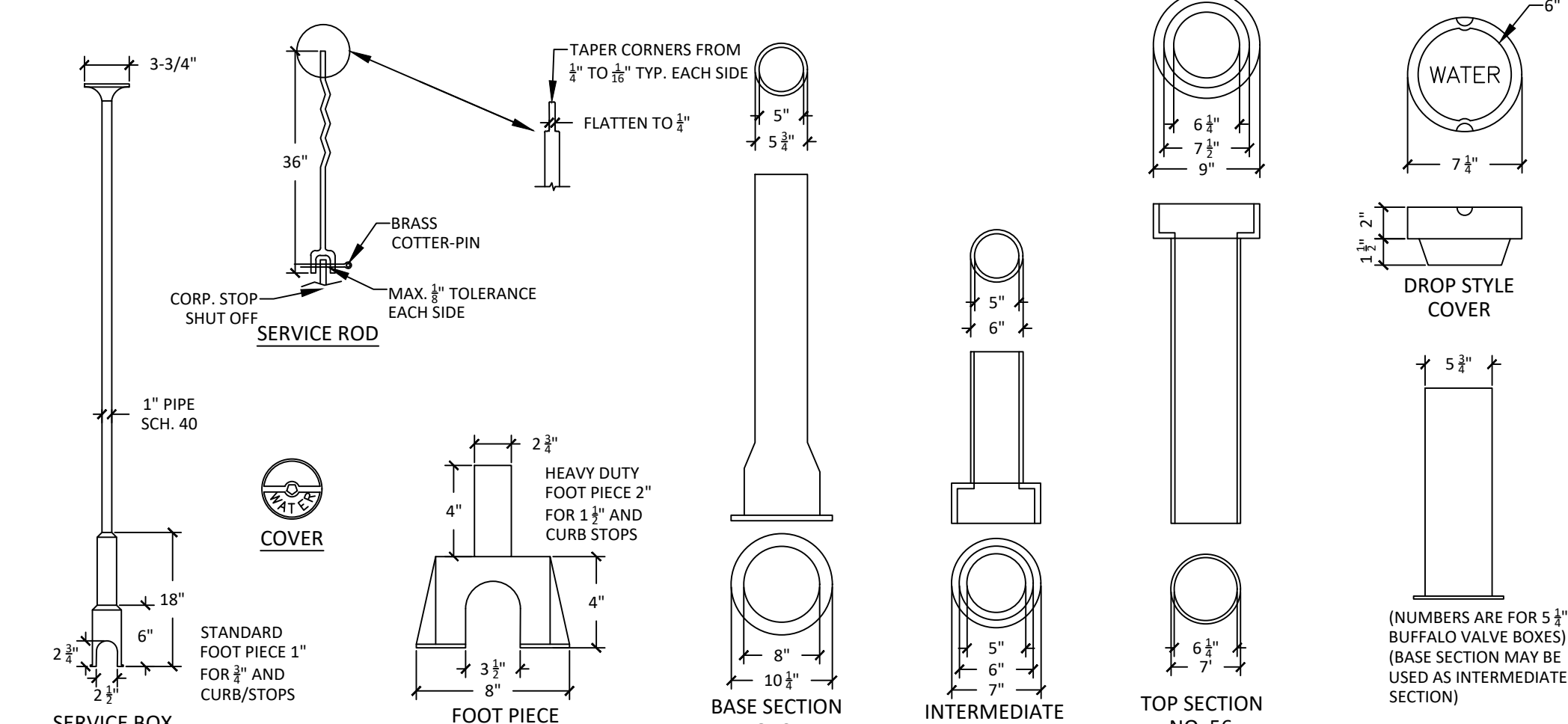
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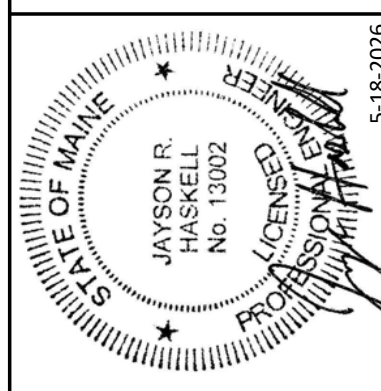
## INSULATION DETAIL



### WATERMAIN TYPICAL TRENCH CROSS-SECTION



VALVE BOX & COVER  
NOT TO SCALE



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WINDHAM, ME 04062  
(207) 501-5055

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## DETAILS

## TURNING LEAF HEIGHTS

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GATEWAY DEVELOPMENT, LLC

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WINDHAM, ME 04062

20013  
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SHEET 18 OF 19

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